

SUNSET CROSSROADS SPECIFIC PLAN



SPECIFIC PLAN NO. 20-20000002

CITY OF BANNING

PUBLIC HEARING DRAFT | SEPTEMBER 2024

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INTRODUCTION

1

CHAPTER ONE establishes the goals and purposes of this Specific Plan, its physical context, its relationship to other regulations and planning documents, and its development goals.



- 1.1 PROJECT SUMMARY
- 1.2 PROJECT LOCATION
- 1.3 EXISTING SITE CHARACTERISTICS
- 1.4 SURROUNDING LAND USES & DEVELOPMENT
- 1.5 DOCUMENT PURPOSE
- 1.6 SPECIFIC PLAN FORMAT
- 1.7 PLANNING APPROACH
- 1.8 SPECIFIC PLAN GOALS
- 1.9 DISCRETIONARY ACTIONS AND APPROVALS

CHAPTER 1 | INTRODUCTION

1.1 PROJECT SUMMARY

Located south of Interstate 10, between Highland Home Road and Sunset Avenue, from the Union Pacific Railroad south to Bobcat Road, the SUNSET CROSSROADS Specific Plan brings considerable industrial business opportunities along with a vibrant retail, recreation and entertainment district to the City of Banning. SUNSET CROSSROADS creates an environment for businesses and economic investments to prosper and where the community can enjoy entertainment, utilize retail services, and realize job opportunities in a variety of business types. At the time this Specific Plan was prepared, the project boundary consists of 554.8 acres under a single ownership, which included an approximately 21-acre existing or future right of way easement for the construction of the Future Sun Lakes Boulevard Extension (which is Not a Part of the Specific Plan). Exclusion of the approximately 21-acre easement for the Future Sun Lakes Boulevard Extension reduces the area subject to the Specific Plan to 533.8 acres. Of the total 533.8 acres within the Specific Plan, 280.1 acres (263.7 acres for development and 16.4 acres for circulation) are located within the City of Banning corporate boundary and 253.7 acres (241.8 acres for development and 11.9 acres for circulation) of the site are located outside of the City of Banning's corporate boundary, within the City's Sphere of Influence. As part of the overall development Project, an application to the Local Agency Formation Commission (LAFCO) was submitted for Annexation of the southern portion of the site into the City of Banning. The Specific Plan will become effective for the southern portion of the site upon annexation of that property into the City of Banning.

SUNSET CROSSROADS establishes 19 Planning Areas with four (4) Land Use Designations, as shown in Table 1-1, *Land Use Summary* and Figure 2-1, *Land Use Plan*, designed to attract new business and new visitors to the City of Banning. The Specific Plan provides a wide range of Industrial, General Commercial, Open Space – Park, and Open Space – Resources land uses to the City, which create new employment opportunities, improve the City's jobs-to-housing balance, and may reduce residents' commute times. The addition of industrial, retail, professional and medical offices, education, recreation, and hospitality uses will also contribute to the City of Banning's sales tax base. As part of the project, the applicant will convey locations for the following infrastructure and municipal uses: a Reverse Osmosis Facility

Site and an Electrical Substation Site. The Electrical Substation Site may be conveyed prior to approval of this Specific Plan. Additionally, sites for a potential future Fire Station and Potable Water Reservoir have been identified.

The 47.9-acre commercial district of SUNSET CROSSROADS (Planning Area 1) stretches along the northeast portion of the site, between Lincoln Street and the project boundary, creating the opportunity for a Maximum Building Square Footage of 268,400 square feet of leisure, retail, medical office, professional office, education, recreation and entertainment uses along with a freeway accessible 125-room hotel and a complete Travel Center including retail and convenience sales, a drive-through fast food restaurant, 20 vehicle fueling stations and six (6) commercial fueling stations. Maximum Building Square Footage is defined as the total square footage of all floors in structure as measured



from either the interior surface of each exterior wall of the structure or, if the structure does not have walls, from each outer edge of the roof. SUNSET CROSSROADS is designed to be a regional commercial and entertainment attraction, providing multi-generational destinations with a focus on activity and wellness-based retail uses which may include but is not limited to stationary wave surfing, indoor skydiving, indoor go-karting, obstacle gyms, and a large indoor/outdoor climbing wall, yoga studios, dance studios and other athletic activities, provided that the City determines that the environmental impacts of such uses do not increase the project's environmental impacts evaluated in the Sunset Crossroads Specific Plan EIR or supplemental environmental analysis is performed as required by CEQA.



SUNSET CROSSROADS establishes a Maximum Building Square Footage of 5,545,000 square feet of Industrial land uses (Planning Areas 2-10) on 392.0 acres, creating opportunities for a wide range of industrial endeavors within the City of Banning. Due to the site's proximity to the Sunset Avenue Interstate 10 interchange, the SUNSET CROSSROADS Specific Plan is particularly well-suited for warehousing and distribution operations, as well as other industrial uses.

Three natural drainage courses, Smith Creek, Pershing Creek, and Highland Wash, (40.5acres) designated Open Space – Resource, traverse the SUNSET CROSSROADS Specific Plan site from northwest to southeast (Planning Areas 13-19), and will

remain undisturbed, except for required road and infrastructure crossings. In addition, along Highland Home Road, the SUNSET CROSSROADS Specific Plan establishes a total of 25.1 acres of open space uses (Planning Areas 11 and 12), which provide parkland and open space uses while buffering the site's industrial development from the residential community to the west. Planning Area 11 is designated Open Space-Parks and consists of a 5.0-acre public park developed with a tot-lot, picnic tables, restrooms and walking trails, the remainder of this 12.6-acre site may contain pedestrian trails, but will otherwise remain undeveloped open space. Planning Area 12 consists of 12.5 acres designated Open Space – Resource, which provides an open space buffer which may contain trails, as well as providing location for a possible Reverse Osmosis Facility Site and Fire Station site.

Table 1-1 LAND USE SUMMARY

| PA | LAND USE DESIGNATION | ACRES |
|----|---------------------------------------|--------------|
| 1 | GENERAL COMMERCIAL | 47.9 |
| 2 | INDUSTRIAL | 33.8 |
| 3 | INDUSTRIAL | 7.8 |
| 4 | INDUSTRIAL | 194.3 |
| 5 | INDUSTRIAL | 66.3 |
| 6 | INDUSTRIAL | 28.1 |
| 7 | INDUSTRIAL | 25.8 |
| 8 | INDUSTRIAL | 16.2 |
| 9 | INDUSTRIAL | 3.3 |
| 10 | INDUSTRIAL | 16.4 |
| | INDUSTRIAL SUBTOTAL | 392.0 |
| | DEVELOPMENT TOTAL | 439.9 |
| 11 | OPEN SPACE - PARKS | 12.6 |
| 12 | OPEN SPACE - RESOURCE | 12.5 |
| 13 | OPEN SPACE - RESOURCE | 7.5 |
| 14 | OPEN SPACE - RESOURCE | 7.1 |
| 15 | OPEN SPACE - RESOURCE | 6.0 |
| 16 | OPEN SPACE - RESOURCE | 4.8 |
| 17 | OPEN SPACE - RESOURCE | 4.1 |
| 18 | OPEN SPACE - RESOURCE | 3.3 |
| 19 | OPEN SPACE - RESOURCE | 7.7 |
| | OPEN SPACE - RESOURCE SUBTOTAL | 53.0 |
| | CIRCULATION | 28.3 |
| | PROJECT TOTAL | 533.8 |

Note: Acreage North of the SLB Extension = 280.1 AC, and South of the SLB Extension = 253.7 AC SLB Extension Easement = approximately 21 AC

The SUNSET CROSSROADS Specific Plan guides development by establishing the distribution, location and extent of the uses of land, including open space, within the area covered by the plan. Land Use Designations (Chapter 2, Development Plan), Infrastructure Plans (Chapter 2, Development Plan), Development Standards (Chapter 3), Design Guidelines (Chapter 4), and Implementation Regulations (Chapter 5, Implementation Plan) address permitted uses, building placement, architectural style, landscaping materials and other design and development elements. These design elements create visual interest, unity, and thematic consistency through the use of complementary exterior building materials, colors, wall and façade treatments. In addition, a conceptual landscaping program for the entire SUNSET CROSSROADS Specific Plan ties together the hardscape and softscape elements of the built environment to create a cohesive visual theme which blends this employment and entertainment center into the Banning community. The project design features contemporary architecture, lighting and signage, combined with architectural elevations and a variety of uses intended to ensure that the SUNSET CROSSROADS Specific Plan contributes to the long-term prosperity of the City of Banning.

Specific Plan circulation and infrastructure improvements are in conformance with the City's General Plan and the plans of local utility purveyors. Local access and connection to the regional transportation network is provided via Sunset Avenue, to the east of the project site. In addition to roads, master-planned on-site infrastructure improvements include potable water, wastewater, reclaimed water lines, and stormwater management, including water quality management/ flood control systems. Additionally, land has been allocated in PA 3, 7, and 12 for the development of municipal facilities, to be constructed by the City of Banning (a Reverse Osmosis Facility, Fire Station site, Potable

Water Reservoir site, and an Electrical Substation). The Specific Plan ensures that necessary roads, utilities, landscape elements, and other infrastructure facilities are installed when needed to accommodate the needs of the Specific Plan as it builds out.

1.2 PROJECT LOCATION

As illustrated on Figure 1-1, *Regional Map*, SUNSET CROSSROADS is located in the City of Banning, in western Riverside County. The City of Banning is served by Interstate 10 (I-10), a major east-west regional transportation corridor which connects Banning to the rest of Riverside County and the greater Los Angeles area, including the Ports of Long Beach and Los Angeles, both major gateways for international trade.

As illustrated on Figure 1-2, *Vicinity Map*, access to SUNSET CROSSROADS is provided via Interstate 10, from the Sunset Avenue interchange located approximately $\frac{1}{4}$ mile to north. SUNSET CROSSROADS stretches east to west along the southern side of the Union Pacific Railroad right of way, between Sunset Avenue and Highland Home Road. North to south, the site extends from the Union Pacific Railroad right of way to Bobcat Road and is bisected by Westward Avenue, which is currently unpaved and unimproved. The Future Sun Lakes Boulevard Extension (SLB Extension) will incorporate the current right-of-way of Westward Avenue. Upon construction, the SLB Extension will connect Sunset Avenue to Highland Home Road. Within this document, references to the SLB Extension include the existing Westward Avenue right-of-way, regardless of which road exists at the time of development. Additional access to the site is provided by Lincoln Street which is currently unpaved and unimproved and planned to be constructed by the Developer as part of the buildup of the Specific Plan, which extends west from Sunset Avenue and terminates within the site.

1.3 SURROUNDING LAND USES AND DEVELOPMENT

As illustrated on Figure 1-3, *Surrounding Land Uses*, the Sun Lakes Community, an established age-restricted residential community and golf course, is located west of Highland Home Road and SUNSET CROSSROADS. The planned Rancho San Gorgonio development which includes planned residential and commercial uses, the Mount San Jacinto College San Gorgonio Pass Campus, and undeveloped land are located east of Sunset Avenue. Properties to the south and southwest include scattered rural residential lots, agricultural operations, and vacant land. The Union Pacific Railroad borders the northern boundary of SUNSET CROSSROADS, with Interstate 10 (I-10) located immediately north of the Railroad.

1.4 EXISTING SITE CHARACTERISTICS

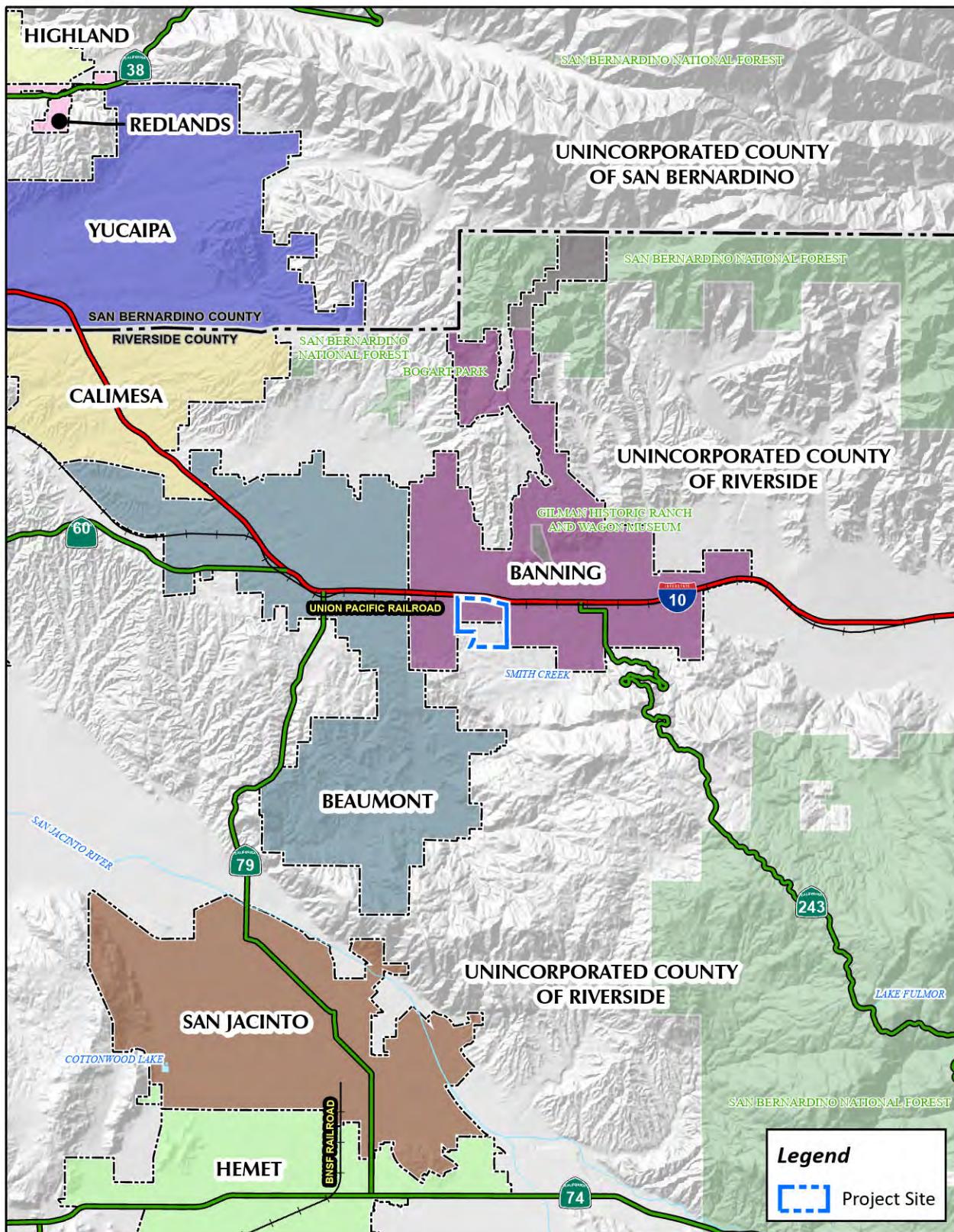
As shown on Figure 1-4, *Pre-Development Topographic Map*, the SUNSET CROSSROADS site is located within the San Gorgonio Pass and is comprised of gently sloping topography, with elevations ranging from approximately 2,399 feet above mean sea level (MSL) at the southeastern corner to 2,523 feet MSL at the northwestern corner. Vegetation cover reflects a history of dry farming and cattle grazing disturbances over a majority of the site, and consists primarily of non-native grasslands, with patches of Riverside sage scrub, mixed ornamental woodland, and sandy wash. The sandy wash habitat is associated with three deeply-incised drainage courses that flow through the site.

Approximately 280.1 acres of the site are within the jurisdictional limits of the City of Banning and approximately 253.7 acres lie within the Sphere of Influence of the City of Banning in unincorporated Riverside County. As part of the Project, an application to the Local Agency Formation Commission (LAFCO) was submitted for Annexation of the 253.7 acres into the City of Banning. The northern portion of the Specific Plan is located within the southern half of Section 7, Township 3 South and Range 1 East. The southern portion of the Specific Plan under is located in the northern half of Section 18, Township 3 South and Range 1 East.

1.5 DOCUMENT PURPOSE

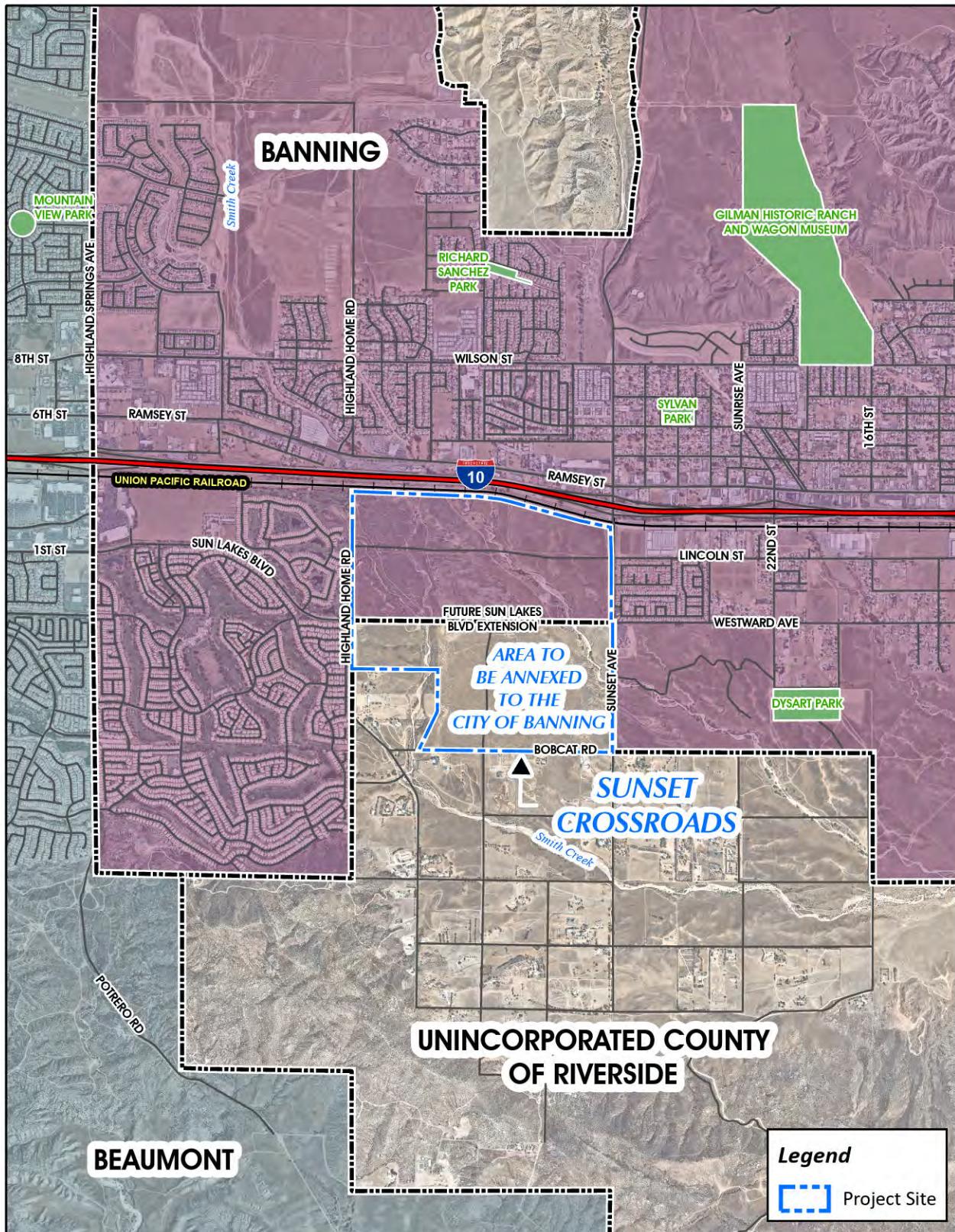
Authorized by California Government Code § 65450 et seq., a Specific Plan is a tool that is used for the systematic implementation of the General Plan for all or part of the area covered by the General Plan. It effectively establishes a link between implementing policies of the General Plan and the individual development proposals in a defined area. As such, this document provides the City of Banning with policies and regulations to ensure efficient, orderly development of the subject property in accordance with the City's adopted General Plan.

The SUNSET CROSSROADS Specific Plan establishes standards for the development of a master planned industrial and general commercial center in the City of Banning. This Specific Plan includes Implementation Regulations relative to land uses and building intensity, as well as Design Guidelines that are intended to provide for innovation in architecture, landscaping, and building arrangements, as future applications submitted within this Specific Plan are proposed to develop within the Specific Plan. All future implementing actions (Design Reviews, Conditional Use Permits, Subdivision Maps, and other similar entitlements) for property located within the boundaries of this Specific Plan are required to be generally consistent with the Implementation Regulations and Design Guidelines set forth in this document and with all applicable City regulations, unless otherwise determined by the Community Development Director. The "Implementation Regulations" of this Specific Plan consist of the General Development Criteria (Section 3.1), Infrastructure Improvement Standards (Chapter 2), Permitted, Conditionally Permitted, Ancillary, and Prohibited Uses (Section 3.2), Development Standards (Sections 3.3 and 3.4), Planning Area Standards (Section 3.5) and Implementation Plan (Section 5) which together constitute the zoning regulations for the SUNSET CROSSROADS Specific Plan. Where conflicts exist between the Implementation Regulations contained in the SUNSET CROSSROADS Specific Plan and those found in the City's Zoning Ordinance, the standards in this Specific Plan shall take precedence.



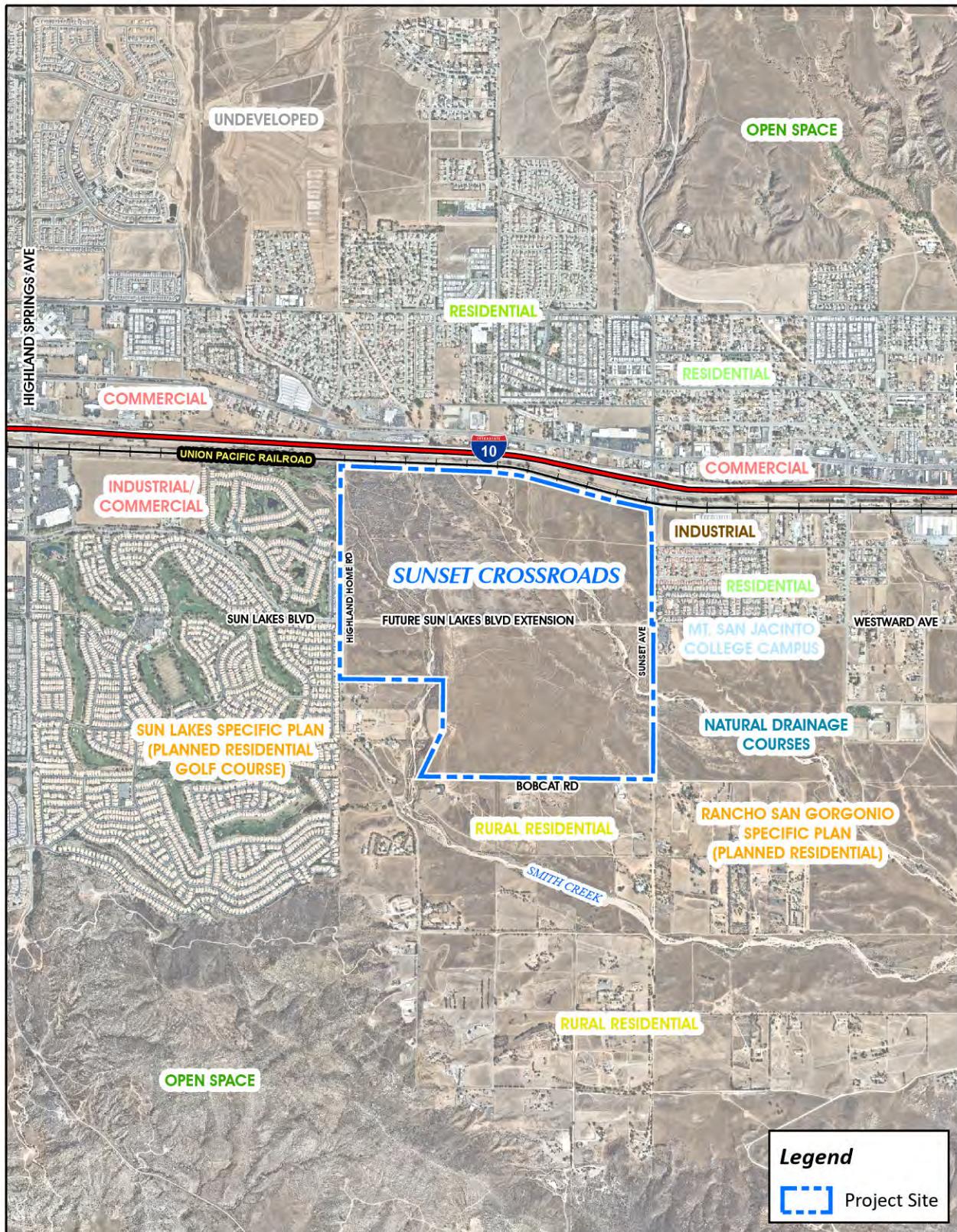
Source(s): ESRI, RCTLMA (2021), Bureau of Land Management (2021)

FIGURE 1-1



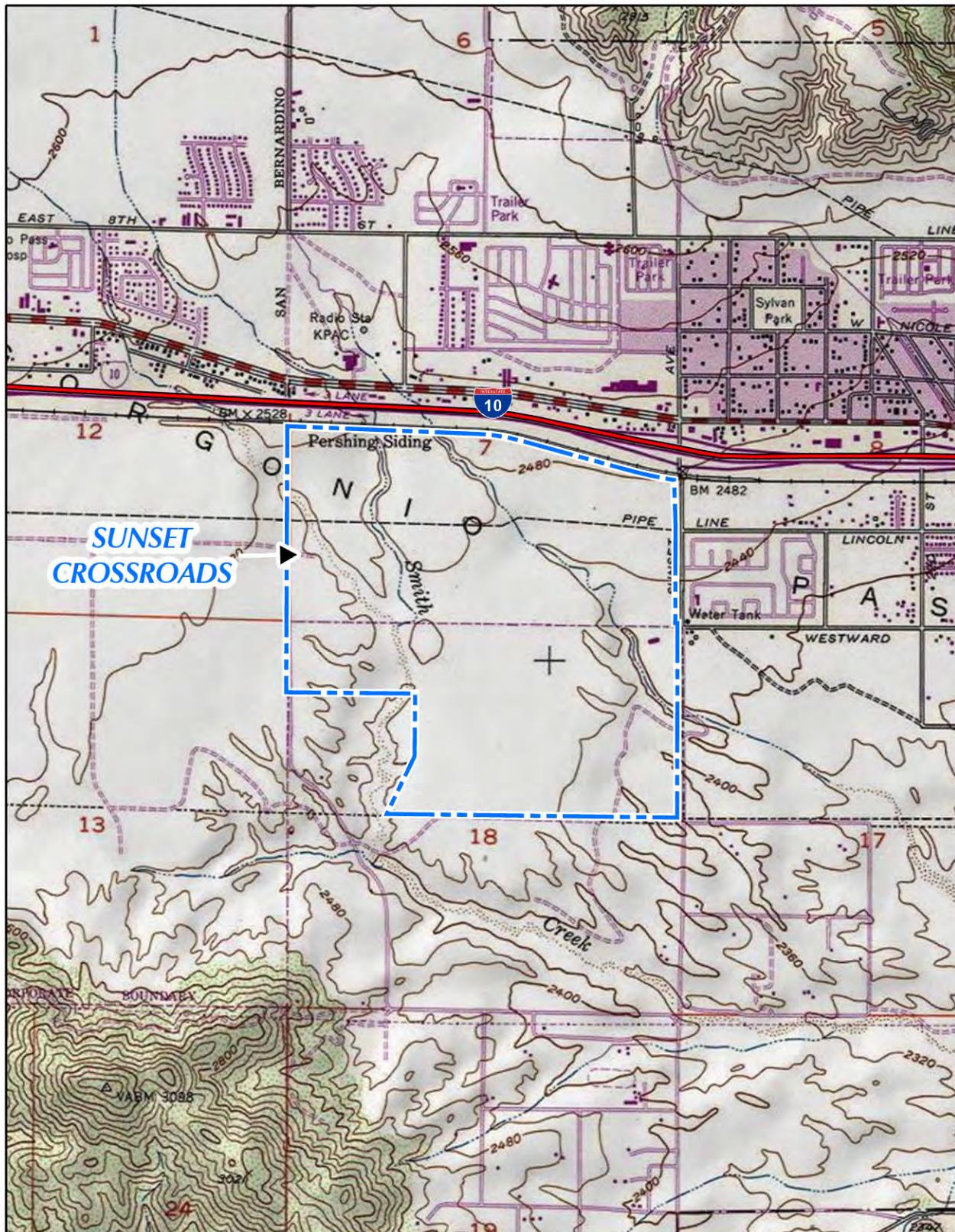
Source(s): NearMap Imagery (2022), RCTLMA (2021)

FIGURE 1-2



Source(s): NearMap Imagery (2022), RCTLMA (2021)

FIGURE 1-3



Source(s): USGS (2013)

FIGURE 1-4

1.6 SPECIFIC PLAN FORMAT

The SUNSET CROSSROADS SPECIFIC PLAN is divided into the following sections: 1) Introduction; 2) Development Plan; 3) Development Standards; 4) Design Guidelines; and 5) Implementation Plan. This SUNSET CROSSROADS SPECIFIC PLAN has been prepared to the provisions of California Government Code §65450, which grants local government agencies the authority to prepare specific plans of development for any area covered by a General Plan for the purpose of establishing systematic methods of implementation of the agency's General Plan. California Government Code §65450 through §65454 establishes the authority to adopt a Specific Plan, identify the required content of a Specific Plan, and mandate consistency with the General Plan. According to § 65450, a Specific Plan shall include text and a diagram or diagrams which specify all of the following details:

1. The distribution, location, and extent of the uses of land, including open space, within the area covered by the Specific Plan;
2. The proposed distribution, location, and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the area covered by the plan and needed to support the land uses described in the Specific Plan;
3. Standards and criteria by which development will proceed, and standards for the conservation, development, and utilization of natural resources, where applicable;
4. A program of implementation measures including regulations, programs, public works projects, and financing measures; and
5. A statement of the relationship of the Specific Plan to the General Plan.

California state law also provides for the inclusion of any other subject that, in the judgement of the local planning agency, is deemed necessary or desirable to implement the General Plan, such as architectural or landscaped design guidelines.

In response to government requirements, this Specific Plan has been prepared to provide the essential link to the policies of the City of Banning General Plan. By functioning as a regulatory document, the SUNSET CROSSROADS SPECIFIC PLAN provides a means of implementing and detailing the City's General Plan and tailoring its policies to the subject property. This Specific Plan is designed to address site specific issues such as building setbacks and visual appearance, as well as community-wide concerns such as vehicular and non-vehicular circulation, energy conservation, landscaping, and the provision for infrastructure improvements. The SUNSET CROSSROADS SPECIFIC PLAN also ensures that new development meets or exceeds City standards for environmental protection, infrastructure, and aesthetic quality.

1.7 PLANNING APPROACH

The SUNSET CROSSROADS Specific Plan was created using an integrated, multi-disciplinary approach. The team assembled to undertake this project included the City, land planners, civil engineers, environmental resource specialists, historians, architects, landscape architects, geotechnical specialists, hydrology specialists, and market analysts. This team worked together to create a comprehensive development plan for the approximately 533.8-acre site, which provides land uses that take advantage of the site's proximity to access to Interstate 10 and responds to economic



trends in the City of Banning. The team also took great care to ensure that the Specific Plan is in conformance with the intent, goals and policies set forth by the City of Banning General Plan.

1.8 SPECIFIC PLAN GOALS



A wide range of key issues were reviewed, analyzed and considered during the preparation of this Specific Plan. Engineering feasibility, market acceptance, economic viability, City General Plan goals, and local community goals were all considered during the planning process. Planning and development goals for the Specific Plan have been established to ensure the functional integrity, economic viability, environmental sensitivity, and positive aesthetic impact of this Specific Plan. With these specific goals in mind, the following goals have been established for the SUNSET CROSSROADS Specific Plan:

- ❖ Create a Specific Plan for the development of state-of-the-art commercial and industrial land uses that accommodate a variety of modern industrial, business, hospitality and commercial activities.
- ❖ Anticipate market demand by providing a mixture of industrial and commercial land uses in a master-planned development that will be marketable and financially profitable within the evolving economic profile of the City of Banning.
- ❖ Provide a mix of commercial and industrial uses that create new job opportunities and reduce the need for City residents to commute outside of the City of Banning for employment, thereby improving the City's jobs to housing ratio.
- ❖ Establish a mix of commercial and industrial uses that result in economic benefits to the City through increased sales taxes and other revenue which can be used to provide City services.
- ❖ Maintain the integrity of the natural environment through the preservation and conservation of open space and natural drainage courses.
- ❖ Locate businesses that rely on transportation efficiency in a location offering superior access to the local and regional transportation network.
- ❖ Identify capital improvements for potable water, reclaimed water, sewer, drainage and water quality, and circulation facilities that serve planned land uses within and adjacent to SUNSET CROSSROADS.
- ❖ Establish a unified thematic concept for SUNSET CROSSROADS through design elements such as architecture, monumentation, theme walls, landscaping using a long-range comprehensive planning approach that cannot be accomplished on a parcel-by-parcel basis.
- ❖ Create a development-wide landscape concept that features drought-tolerant plant materials to provide for an aesthetically pleasing outdoor environment, while minimizing the demand for water resources.

1.9 DISCRETIONARY ACTIONS AND APPROVALS

Because the southern half of the Specific Plan (253.7 acres) is located within the jurisdictional boundaries of the County of Riverside and within the City of Banning's Sphere of Influence, an Annexation to the City will be required for development within the SUNSET CROSSROADS Specific Plan. A separate action is required by the Riverside County Local Agency Formation Commission (LAFCO) prior to formal Annexation of this portion of the site into the City of Banning. This Specific Plan has been prepared under the authority of the City of Banning Planning Department, which is also the Lead Agency for SUNSET CROSSROADS' approvals. This document will be used by the City in connection with the following decisions:

City of Banning Planning Commission

- ❖ Recommendation to the City Council regarding certification of the Project EIR No. 2021020011.
- ❖ Recommendation to the City Council adopt the General Plan Amendment No. 20-2504.
- ❖ Recommendation to the City Council regarding the adoption of Pre-Zoning of Specific Plan No. 20-20000002 by Ordinance.
- ❖ Recommendation to the City Council regarding the adoption of a Development Agreement.
- ❖ Recommendation to the City Council regarding approval of Subdivision Map(s).
- ❖ Recommendation to the City Council regarding the Annexation of the Specific Plan into the City of Banning.

City of Banning City Council

- ❖ Certification of the Project EIR No. 2021020011.
- ❖ Adoption by Resolution of General Plan Amendment No. 20-2504.
- ❖ Adoption by Ordinance of Pre-Zoning of Specific Plan No. 20-20000002.
- ❖ Approval by Ordinance of a Development Agreement.
- ❖ Approval of Subdivision Map(s).
- ❖ Approval by Resolution of an Annexation of the Specific Plan into the City of Banning.

Riverside Local Agency Formation Commission (LAFCO)

- ❖ Annexation of approximately 254 acres of the Specific Plan site from unincorporated Riverside County into the City of Banning.

Subsequent discretionary actions may include, but not be limited to, Annexations, Final Maps, Conditional Use Permits, Design Reviews, Subdivision Maps, Grading Permits, water and sewer system approvals, encroachment permits, and state and federal resource agency permits and approvals.

DEVELOPMENT PLAN

2

CHAPTER Two provides descriptions of the SUNSET CROSSROADS Land Use Plan, the circulation system, and the plans for infrastructure including, but not limited to potable water, recycled water, sewer, drainage and water quality, grading, and fire protection.



- 2.1 LAND USE PLAN
- 2.2 CIRCULATION PLAN
- 2.3 POTABLE WATER PLAN
- 2.4 RECYCLED WATER PLAN
- 2.5 SEWER PLAN
- 2.6 DRAINAGE AND WATER QUALITY PLAN
- 2.7 GRADING PLAN
- 2.8 OPEN SPACE PLAN
- 2.9 FIRE PROTECTION PLAN

CHAPTER 2 | DEVELOPMENT PLAN

2.1. LAND USE PLAN

2.1.1. DEVELOPMENT CONCEPT

The SUNSET CROSSROADS Specific Plan provides for the development of a modern Industrial and General Commercial development on approximately 533.8 acres (excluding the approximately 21-acre easement for the Sun Lakes Boulevard Extension). Complemented by a cohesive architectural and landscape design theme and within view of commuters along Interstate 10 (I-10), SUNSET CROSSROADS establishes itself as a premier, state-of-the-art commerce center within the City of Banning.

The commercial center provides the City with an opportunity to develop a gathering site for the community at SUNSET CROSSROADS with a variety of leisure, retail, medical office, professional office, education, wellness, recreation, entertainment, restaurant uses, and other retail uses along with a freeway accessible 125-room hotel and a Travel Center. The project envisions a large communal outdoor gathering space as a primary focal point, which can accommodate a variety of recreational activities.

Sports-oriented activities such as stationary wave surfing, indoor skydiving, indoor go-karting, obstacle gyms, and a large indoor/outdoor climbing wall are examples of potential recreational uses that may develop in the commercial area. It is the mixture of unique wellness-based uses, restaurants, and other commercial uses complimented by outdoor gathering spaces and community focal points that are intended to serve as the core of SUNSET CROSSROADS' commercial attractions for years to come. Planning Area 1 includes a 125-room hotel of approximately 90,000 square feet.¹ The proposed Travel Center would provide access to quick fuel and food options conveniently located adjacent to the I-10. With development of the proposed hotel and Travel Center, the commercial center would accommodate the hospitality needs of visitors to the City, as well as travelers along the I-10 and contribute to the long-term economic vitality of the City by generating sales and transient bed tax revenue.



The industrial part of SUNSET CROSSROADS spreads across 392.0 acres of the site, providing a Maximum Building Square Footage of 5,545,000 square feet of space for manufacturing, storage, warehouse, and various other uses. Due to the site's proximity to the Interstate 10 Sunset Avenue interchange, the SUNSET CROSSROADS Specific Plan is particularly well-suited for warehousing and distribution operations, as well as other industrial uses.

With access to the existing major transportation network, SUNSET CROSSROADS establishes a premier location and atmosphere designed to attract economic investment to this part of the City while also creating substantial employment

¹ The hotel's approximate square footage is not included in the Maximum Building Square Footage 268,400 square feet.

opportunities for City residents. For local residents, employment opportunities close to home mean reduced or eliminated commutes, and an improved quality of life. The high-quality design of SUNSET CROSSROADS includes amenities for employees and visitors, such as parks, recreation-based businesses, a sidewalk system, and outdoor gathering areas.

This SUNSET CROSSROADS Specific Plan includes a detailed Circulation Plan organized to ensure efficient access to individual tenant, open space, and “back of house” areas. Direct access from the I-10 to the Specific Plan circulation system is provided via Sunset Avenue. Lincoln Street provides access to the commercial area and Industrial uses within Planning Area 7. Future Sun Lakes Boulevard Extension (SLB Extension) and Bobcat Road provide access to the Industrial uses. Highland Home Road along the Specific Plan’s western boundary provides connection to the SLB Extension and secondary access to Planning Area 7.

The three natural drainage courses, Smith Creek, Pershing Creek, and Highland Wash, totaling 40.5 acres which traverse the SUNSET CROSSROADS Specific Plan site from northwest to southeast (Planning Areas 13 through 19), are designated Open Space – Resource and will remain undisturbed except for required road and infrastructure crossings. In addition, the 12.5 acres of Open Space – Resource in Planning Area 12 and the 12.6 acres of Open Space – Parks in Planning Area 11, combined with the right of way of Highland Home Road buffers the site’s industrial development from the Sun Lakes community to the west. Planning Area 11 is designated Open Space-Parks and includes a 5.0-acre public park developed with a tot-lot, picnic tables, restrooms and walking trails, while the remainder of the 12.6-acre site is reserved for Open Space and trails. Planning Area 12 establishes 12.5 acres designated Open Space – Resource, which may include a 2.3-acre Reverse Osmosis Facility Site, Fire Station Site, and pedestrian trails. Additionally, to reduce the effect of its industrial uses on the existing residential community to the west, a minimum 100-foot-wide open space buffer is required between industrial buildings in PA 7 and the western project boundary. The City may construct an electrical substation at the northwest corner of PA 7, which would be exempt from the 100-foot setback requirement.

The physical arrangement of Land uses, acreages, Planning Areas and the major roadways within the SUNSET CROSSROADS Specific Plan are depicted in the SUNSET CROSSROADS Land Use Plan (Figure 2-1, *Land Use Plan*). Table 2-1, *Land Use Statistical Summary*, lists each Planning Area by land use type on the Land Use Plan and provides the General Plan Land Use Designation and acreage for each Planning Area and the overall Maximum Building Square Footage for each General Plan Land Use Designation. Refer to Chapter 3, *Development Standards*, and the Infrastructure Improvement Standards in Chapter 2 for the Specific Plan Land Use and Implementation Regulations applicable to each Planning Area. Project Implementation information is contained in Chapter 5, *Implementation Plan*.

Table 2-1 Land Use Plan Statistical Summary

| PA | LAND USE DESIGNATION | ACRES | MAXIMUM BUILDING SQUARE FOOTAGE ¹ |
|---------------------------------------|-----------------------|---------------|--|
| 1 | GENERAL COMMERCIAL | 47.9 | 268,400 ^{2,3} 5,545,000 |
| 2 | INDUSTRIAL | 33.8 | |
| 3 | INDUSTRIAL | 7.8 | |
| 4 | INDUSTRIAL | 194.3 | |
| 5 | INDUSTRIAL | 66.3 | |
| 6 | INDUSTRIAL | 28.1 | |
| 7 | INDUSTRIAL | 25.8 | |
| 8 | INDUSTRIAL | 16.2 | |
| 9 | INDUSTRIAL | 3.3 | |
| 10 | INDUSTRIAL | 16.4 | |
| INDUSTRIAL SUBTOTAL | | 392.0 | |
| DEVELOPMENT TOTAL | | 439.9 | 5,813,400 |
| 11 | OPEN SPACE - PARKS | 12.6 | N/A |
| 12 | OPEN SPACE - RESOURCE | 12.5 | |
| 13 | OPEN SPACE - RESOURCE | 7.5 | |
| 14 | OPEN SPACE - RESOURCE | 7.1 | |
| 15 | OPEN SPACE - RESOURCE | 6.0 | |
| 16 | OPEN SPACE - RESOURCE | 4.8 | |
| 17 | OPEN SPACE - RESOURCE | 4.1 | |
| 18 | OPEN SPACE - RESOURCE | 3.3 | |
| 19 | OPEN SPACE - RESOURCE | 7.7 | |
| OPEN SPACE - RESOURCE SUBTOTAL | | 53.0 | |
| OPEN SPACE TOTAL | | 65.6 | |
| | CIRCULATION | 28.3 | |
| | PROJECT TOTAL | 533.84 | 5,813,400 |

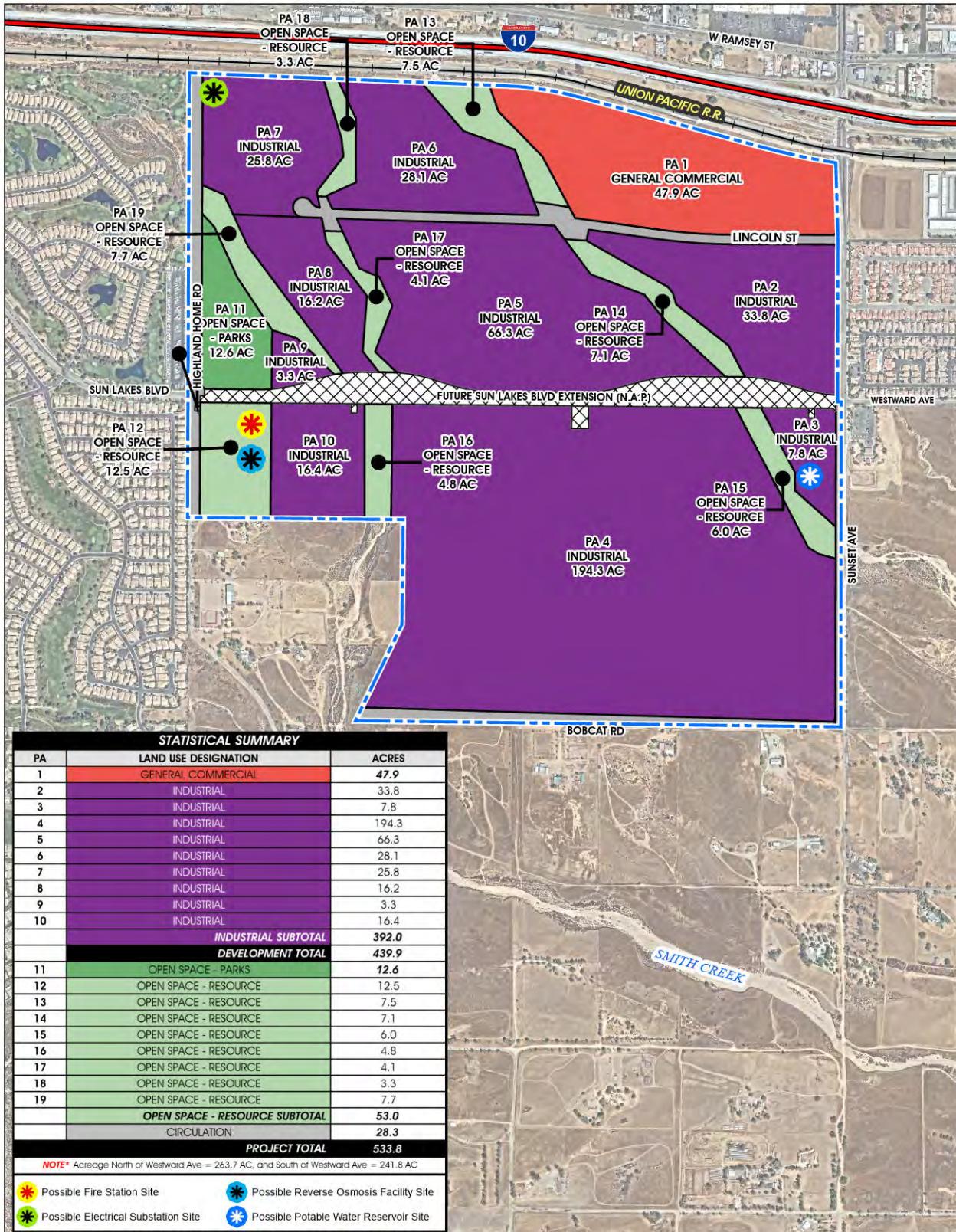
Notes:

¹ Maximum Building Square Footage is a combined total for each Land Use Designation and is not calculated on a Planning Area-by-Planning Area basis. Maximum Building Square Footage is defined as the total square footage of all floors in structure as measured from either the interior surface of each exterior wall of the structure or, if the structure does not have walls, from each outer edge of the roof.

² Planning Area 1 is anticipated to include a 125-room limited-service hotel (approximately 90,000 square feet). The 90,000 square feet of hotel use is not counted as part of the General Commercial's 268,400 Maximum Building Square Footage. With the hotel's approximately 90,000 square feet, the approximate total square footage in Planning Area 1 would be 358,400 square feet. The total square footage of Planning Area 1 excludes the fuel pump area and the hotel square footage (approximately 90,000 square feet) because the project's traffic analysis for the commercial site estimates traffic based on square footage, while the traffic analysis for hotels is based on the number of rooms.

³ Buildings associated with the Travel Center are included in the maximum 268,400 square feet, but fuel pump areas are not.

⁴ Excludes the approximately 21-acre easement for the Sun Lakes Boulevard Extension.



Source(s): NearMap Imagery (2022), RCTLMA (2021)
Composite: Proactive Engineering Consultants West (01-05-2023)

FIGURE 2-1

2.1.2. LAND USE SUMMARY

This Specific Plan Land Use Plan establishes four (4) General Plan Land Use Designations: General Commercial (GC), Industrial (I), Open Space – Parks (OS-P), and Open Space – Resource (OS-R). The SUNSET CROSSROADS Specific Plan is divided into 19 “Planning Areas,” precise geographic areas to which identified Development Standards (Chapter 3), Design Guidelines (Chapter 4) and Implementation Regulations (Chapter 5), are uniformly applied.

The SUNSET CROSSROADS Specific Plan provides for a maximum building space of approximately 5,813,400 square feet and establishes: General Commercial uses on 47.9 acres with a Maximum Building Square Footage of 268,4000 square feet of building space (excluding approximately 90,000 square feet of hotel), Industrial uses on approximately 392.0 acres with a Maximum Building Square Footage of 5,545,000 square feet of building space, and 65.4 acres of Open Space land uses including 12.6 acres of Open Space – Parks and 53.0 acres of Open Space – Resource, which facilitate the existing natural drainage of the site, and 28.3 acres of Circulation.

2.1.3. GENERAL COMMERCIAL (GC) – 47.9 ACRES

Planning Area 1 is an approximately 47.9-acre district permitting a wide range of commercial uses, which may include leisure, retail, medical office, professional office, education, wellness, recreation, entertainment restaurant and other retail uses along with a freeway accessible 125-room hotel and a complete Travel Center including retail and convenience sales, a drive-through fast food restaurant, 20 vehicle fueling stations and commercial fueling stations.

SUNSET CROSSROADS is designed to be a regional commercial and entertainment attraction, providing multi-generational destinations such as activity and wellness-based retail uses including but not limited to stationary wave surfing, indoor skydiving, indoor go-karting, obstacle gyms, and a large indoor/outdoor climbing wall, yoga studios, dance studios and other athletic activities. Sit-down restaurants (with patio areas), a food hall and specialty retail uses that will orient inwards to central open space areas would allow for outdoor activities (i.e., walking paths, grass areas, outdoor displays, benches) or could be used by on-site vendors/concessionaires and as connections between uses along with an internal system of pedestrian walkways. Because the buildings in the commercial and industrial areas of the Development Site will be leased to third parties, the Specific Plan provides for a range of allowable uses. Therefore, while the uses described above may be developed under the Specific Plan, the Specific Plan does not require these uses to be implemented and any of the commercial land uses listed in Table 3-1 are consistent with the Specific Plan and allowed uses so long as the City determines that the environmental impacts of such uses do not increase the project's environmental impacts evaluated in the Sunset Crossroads Specific Plan EIR or supplemental environmental analysis is performed as required by CEQA. The full list of Permitted, Conditionally Permitted, Ancillary, and Prohibited Uses in this Planning Area is provided in Chapter 3, Development Standards. In addition to providing a unique location for the intended leisure, hospitality, and recreational activities, the General Commercial uses within Planning Area 1 provide increased employment opportunities for the City's residents, improves the City's jobs-to-housing balance, and may reduce residents' commute times, while the addition of retail, recreation commercial, and travel and hospitality uses and other permitted uses contribute to the City's sales tax base.



The Implementation Regulations and Design Guidelines set forth in this Specific Plan require buildings in Planning Area 1 to be arranged and designed in a contemporary, pedestrian friendly manner, so that the buildings are inviting and softened by pedestrian scale components and landscape design.

2.1.4. INDUSTRIAL (LI) – 392.0 ACRES

Planning Areas 2 through 10 provide for development of a Maximum Building Square Footage of 5,450,000 square feet of industrial land uses within approximately 392.0 acres within. The acreages for Planning Areas 2 through 10 are provided on Table 2-1, *Land Use Plan Statistical Summary*. The Specific Plan will take advantage of its proximity to the Sunset Avenue interchange at I-10 to facilitate the development of warehousing, distribution, manufacturing and other industrial uses such as general light industrial, manufacturing, parcel hub, warehouse/storage, truck/trailer parking and storage, RV storage, high cube warehouse, cold storage, cold storage warehouse, fulfillment center, and e-commerce operations. To accommodate future tenants, the Specific Plan allows the size of individual buildings to vary within an established range (increasing or decreasing by up to 20%) however, the overall square footage of buildings in the industrial Planning Areas are capped at 5.545 million square feet. Direct access to the proposed industrial uses is provided from Sunset Avenue via Lincoln Street and a variety of internal driveways, Highland Home Road, the SLB Extension, and Bobcat Road (upon completion of the Future Sunset Avenue Bridge). Bobcat Road may be required for secondary access or fire access at earlier project phases.

The Industrial building sites within the Development Project may include outdoor employee break areas with tables affixed to the ground to provide employees with a location to eat, gather, and enjoy being outside. Shading of these areas may be achieved through a combination of shade trees, umbrellas, or man-made shade structures. Other recreational amenities within the Industrial areas may include pedestrian walkways, seating areas, overhead structures, and open space areas. Development of the Industrial Planning Areas shall be subject to the applicable Infrastructure Improvement Standards, Development Standards and Planning Area Standards set forth in the Specific Plan.

Because the buildings in the industrial areas of the Development Site will be leased to third parties, it is not possible to specifically define the uses at this time and therefore the Specific Plan provides for a range of allowable uses. Therefore, while the uses described above may be developed under the Specific Plan, the Specific Plan does not require these uses to be implemented and any of the industrial land uses listed in Table 3-1 are consistent with the Specific Plan and allowed uses so long as the City determines either that such uses do not increase the project's environmental impacts evaluated in the Sunset Crossroads Specific Plan EIR (or in additional supplemental environmental analysis previously performed by the City) or the City performs supplemental analysis as required by CEQA. The full list of Permitted, Conditionally Permitted, Ancillary, and Prohibited Uses in Planning Areas 2-10 are provided in Chapter 3, Development Standards, of the Specific Plan.

2.1.5. PUBLIC FACILITIES

SUNSET CROSSROADS provides location for a 1.0-acre Electrical Substation Site in the northwest corner of Planning Area 7, a 2.3-acre Reverse Osmosis Facility Site and Future Fire Station Site in Planning Area 12, and a Potable Water Reservoir Site in Planning Area 3.

2.1.6. PLANNING AREA 11 OPEN SPACE-PARKS (OS-P) – 12.6 ACRES

Planning Area 11 designates 12.6 acres of Open Space-Parks located at the northeast corner of Highland Home Road and the SLB Extension, including a 5.0-acre passive public park, with the remainder of the 12.6-acre site is reserved for Open Space and trails. As illustrated on Figure 4-4, *Conceptual Park Plan (PA 11)*, the park may provide recreational amenities including, but not limited to a tot lot playground, picnic tables, walking paths, parking, and restrooms, parking lot and security lighting. Open field areas are provided next to the playground and picnic tables, along with a looped walking path which winds through the recreational area surrounded by trees and landscaping.

2.1.7. PLANNING AREA 12 OPEN SPACE-RESOURCE (OS-R) – 12.5 ACRES

Approximately 12.5 acres of the site (Planning Area 12) are reserved for passive open space uses which may include trails. Planning Area 12 may also include a 2.3-acre Reverse Osmosis Facility Site and a Future Fire Station Site. No other development is permitted in Planning Area 12.

2.1.8. PLANNING AREAS 13-19 OPEN SPACE-RESOURCE (OS-R) – 40.5 ACRES

Approximately 40.5 acres of the site (Planning Areas 13 through 19) contain three (3) existing natural drainage watercourses, Smith Creek, Pershing Creek, and Highland Wash, which traverse the Specific Plan in a northwesterly to southeasterly direction. To preserve these areas, they are designated Open Space – Resource and no development or land uses are permitted, except those activities required to cross these areas with roads (Lincoln Street, Highland Home Road) and utilities.

2.2. CIRCULATION PLAN

The SUNSET CROSSROADS Circulation Plan provides direct, safe, and convenient access for visitors, employees, and goods to and from the Specific Plan through a safe and efficient network of roadways.

The project is situated in close proximity to the regional transportation network which connects the site to the Ports of Long Beach and Los Angeles, both major gateways for international trade, the Inland Empire and Coachella Valley, and the Western United States. As illustrated on Figure 2-2, *Conceptual Circulation Plan*, regional access to SUNSET CROSSROADS is provided via I-10 from the Sunset Avenue interchange located approximately $\frac{1}{4}$ mile to the northeast of the Specific Plan. The site is approximately 3.5 miles east of the junction of CA-60 Freeway and Interstate 10 and approximately 22 miles east of Interstate 215. Primary local access to the SUNSET CROSSROADS Specific Plan from the north and south is provided via Sunset Avenue which forms the site's eastern boundary. Additional access is provided by Bobcat Road which forms the site's southern boundary and from Highland Home Road, which forms the site's western boundary.

Sunset Avenue will provide access to the entire site via Lincoln Street and the SLB Extension. Access to Planning Area 1 is provided by Lincoln Street. Direct access to Planning Areas, 2, 3, and 4 is provided by Sunset Avenue. Access to Planning Areas 6, and 7 is provided from the south by Lincoln Street. Access to Planning Areas 2, 5, and 8 is provided from the north by Lincoln Street and by the SLB Extension to the south. Planning Areas 9 and 10 have access from the SLB Extension. Planning Areas 11 and 12 are accessible from the SLB Extension. Access to Planning Area 4 relies on the combination of Sunset Avenue (south of PA 15) and Bobcat Road, or the combination of Sun Lakes Boulevard Extension and Bobcat Road to achieve the required two points of access, and may not be developable until two points of access are available. The land within SLB Extension is subject to an existing or future right of way easement in favor of the City and will become City right-of-way upon Annexation of the southern portion of the site into the City, and is excluded from the Specific Plan regulations (Not A Part). The SLB Extension will incorporate the current right-of-way of Westward Avenue. Construction of the SLB Extension is under the control of the City and could be constructed by the City or by third parties designated by the City as a separate project.

1. Sunset Avenue (Interstate 10 to Lincoln Street) – Arterial Highway (110' ROW)

As depicted on Figure 2-2, *Conceptual Circulation Plan*, the primary access point for SUNSET CROSSROADS is Sunset Avenue, which connects the site to the regional transportation network. The segment of Sunset Avenue between Interstate 10 and the SLB Extension is designated by the City General Plan as an Arterial Highway (110' ROW) and forms the project's eastern boundary. As shown in Figure 2-3, *Conceptual Roadway Cross-Sections*, the 110-foot of right of way comprising Sunset Avenue consists of 68 feet of paving, an 18-foot-wide center raised or painted median, with a 4-foot-wide landscaped parkway and 8-foot-wide parkway-adjacent sidewalk on the west side of the street, and an 8-foot-wide curb-adjacent sidewalk and 4-foot-wide landscaped parkway on the east side of the street. An eight-foot-wide Class II bike lane is provided on both sides of the paving, adjacent to the curb. As a result of the westward shift in alignment of Sunset Avenue between Lincoln Street and SLB Extension, the alignment in this location will also shift to the west from the existing centerline.

2. Sunset Avenue (Lincoln Street to SLB Extension) – Modified Secondary Highway (110' ROW)

As depicted on Figure 2-2, *Conceptual Circulation Plan*, the segment of Sunset Avenue between Lincoln Street and the SLB Extension is designated as a Modified Secondary Highway (110' ROW). As shown in Figure 2-3, *Conceptual Roadway Cross-Sections*, the 110-foot of right of way comprising Sunset Avenue consists of 68 feet of paving, an 18-foot-wide center raised or painted median, with a 4-foot-wide landscaped parkway and 8-foot-wide parkway-adjacent sidewalk on the west side of the street, and a 4-foot-wide parkway and 8-foot-wide parkway-adjacent sidewalk on the east side of the street. An eight-foot-wide Class II bike lane is provided on both sides of the paving, adjacent to the curb. To address traffic noise impacts along Sunset Avenue, the alignment of this portion of Sunset Avenue is shifted to the west from its previously proposed location to provide additional distance from sensitive receptors east of Sunset Avenue. More specifically, the centerline of Sunset Avenue between Lincoln Street and Sun Lakes Boulevard would be adjusted 42 feet to the west from the existing centerline with implementation of the Development Project, new centerline being 72 ft from the nearest residential property line and 115 ft from the school at the MSJC Site. The resulting 17 feet of excess right of way to the east may be landscaped.

3. Sunset Avenue (SLB Extension to Bobcat Road) – Secondary Highway (88' ROW)

As depicted on Figure 2-2, *Conceptual Circulation Plan*, the segment of Sunset Avenue between the SLB Extension and Bobcat Road is designated by the City General Plan as a Secondary Highway (88' ROW). As shown in Figure 2-3, *Conceptual Roadway Cross-Sections*, the 88-foot of right of way comprising Sunset Avenue consists of 64 feet of paving with a 4-foot-wide landscaped parkway and 8-foot-wide parkway-adjacent sidewalk provided on both sides of the street. As a result of the westward shift in alignment of Sunset Avenue between Lincoln Street and SLB Extension, the alignment in this location will also shift to the west from the existing centerline.

4. Bobcat Road – Divided Collector Street (78' ROW)

As depicted on Figure 2-2, *Conceptual Circulation Plan*, Bobcat Road is designated as a Divided Collector Street (78' ROW) and forms the project's southern boundary along the south side of Planning Area 4. As shown in Figure 2-3, *Conceptual Roadway Cross-Sections*, the 78-foot of right of way comprising Bobcat Road consists of 56 feet of paving with a 6-foot-wide landscaped parkway and a 5-foot-wide parkway-adjacent sidewalk on the south side of the street, and a 6-foot-wide curb-adjacent sidewalk and 5-foot-wide landscaped parkway on the north side of the street. The Specific Plan and future applications submitted within this Specific Plan will dedicate and construct the northern half-width plus 10' improvements of Bobcat Road along the project frontage.

To address the potential for impacts to residences on the other side of Bobcat Road from on-site Development Project operations, on-site project operations are revised to require:

1. Cold storage equipment previously allowed on industrial building rooftops will be shielded or relocated to the ground floor; and
2. Construction of 10 ft high “wing walls” on the south end of warehouse buildings in Planning Area 4, and 6 ft high walls that surround the automobile parking lots south of warehouse buildings in Planning Area 4 as depicted in the SoundPLAN printouts in Attachment A to the Supplemental Noise Analysis (Final EIR, Appendix I-2, Attachment A).

5. Lincoln Street – Divided Collector Street (78' ROW)

As depicted on Figure 2-2, *Conceptual Circulation Plan*, Lincoln Street is designated as a Divided Collector Street (78' ROW) within the project, providing access to the project site from Sunset Avenue. As shown in Figure 2-3, *Conceptual Roadway Cross-Sections*, the 78-foot of right of way comprising Lincoln Street consists of 56 feet of paving with a 6-foot-wide landscaped parkway and 5-foot-wide parkway-adjacent sidewalk on the south side of the street and a 6-foot-wide curb-adjacent sidewalk and 5-foot-wide landscaped parkway on the north side of the street. An eight-foot-wide Class II bike lane is provided on both sides of the paving, adjacent to the curb. The Specific Plan and future applications submitted within this Specific Plan will construct full-width improvements along the length of Lincoln Street on the project site. Lincoln Street will cross two existing natural drain courses via box culverts to be constructed by the project and maintained by the City. In Planning Area 7, a 24'-wide Secondary Access Connection will link Lincoln Street to Highland Home Road via a private roadway.

6. Highland Home Road (north of SLB Extension)– Modified Secondary Highway (66' ROW)

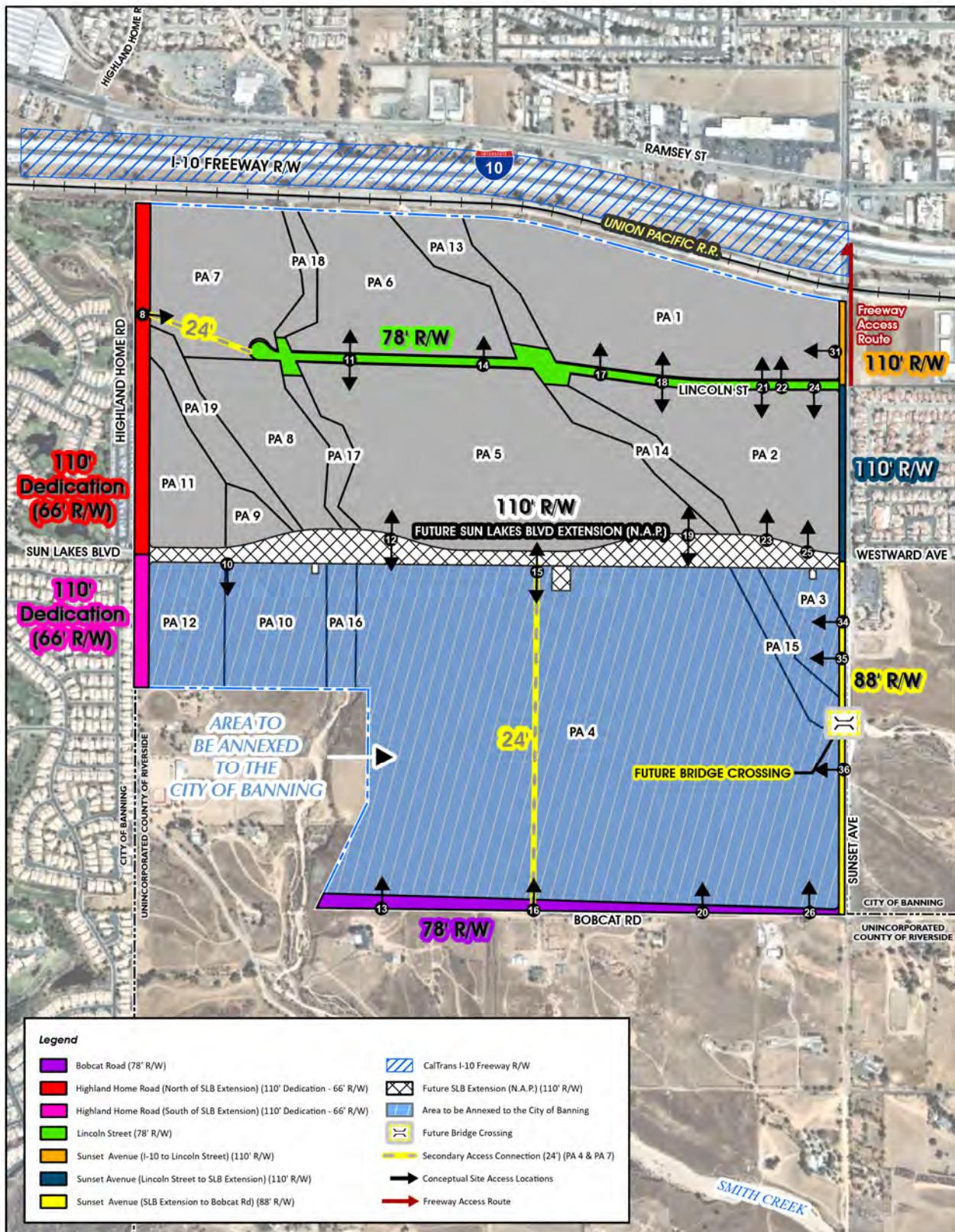
As depicted on Figure 2-2, *Conceptual Circulation Plan*, the segment of Highland Home Road north of the SLB Extension is designated as a Modified Secondary Highway (66' ROW) within the project. Highland Home Road forms a portion of the Specific Plan's western boundary. As shown in Figure 2-3, *Conceptual Roadway Cross-Sections*, Highland Home Road consists of a 66-foot of right of way within a 110-foot dedication. The Specific Plan and future applications submitted within this Specific Plan will dedicate the full 110-foot width of Highland Home Road as from the existing eastern property boundary of the Sun Lakes community. The Specific Plan will construct the full width (66 feet) of Highland Home Road with a 5-foot-wide curb-adjacent sidewalk, a 6-foot-wide landscaped parkway, and 44 feet of paving along the western boundary of the right of way, from the SLB Extension to Planning Area 7, where it will terminate in a cul-de-sac and provide secondary access to Planning Area 7 via a 24' wide Secondary Access Connection.

7. Highland Home Road (south of SLB Extension)– Modified Collector Street (66' ROW)

As depicted on Figure 2-2, *Conceptual Circulation Plan*, the segment of Highland Home Road south of the SLB Extension is designated as a Modified Collector Street (66' ROW) within the project. Highland Home Road forms a portion of the Specific Plan's western boundary. As shown in Figure 2-3, *Conceptual Roadway Cross-Sections*, Highland Home Road consists of a 66-foot right of way within a 110-foot dedication. The Specific Plan and future applications submitted within this Specific Plan will dedicate the full 110-foot width of Highland Home Road from the existing eastern property boundary of the Sun Lakes community. The Specific Plan will construct the remaining half-width (33 feet) of Highland Home Road including a 5-foot Class III bikeway, a 6-foot-wide curb-adjacent sidewalk, and a 5-foot-wide landscaped parkway to complete the road section based on existing curb location adjacent to the Sun Lakes Community, from the southwest corner of PA 12 to the SLB Extension. Truck traffic shall be restricted on Highland Home Road south of the SLB Extension.

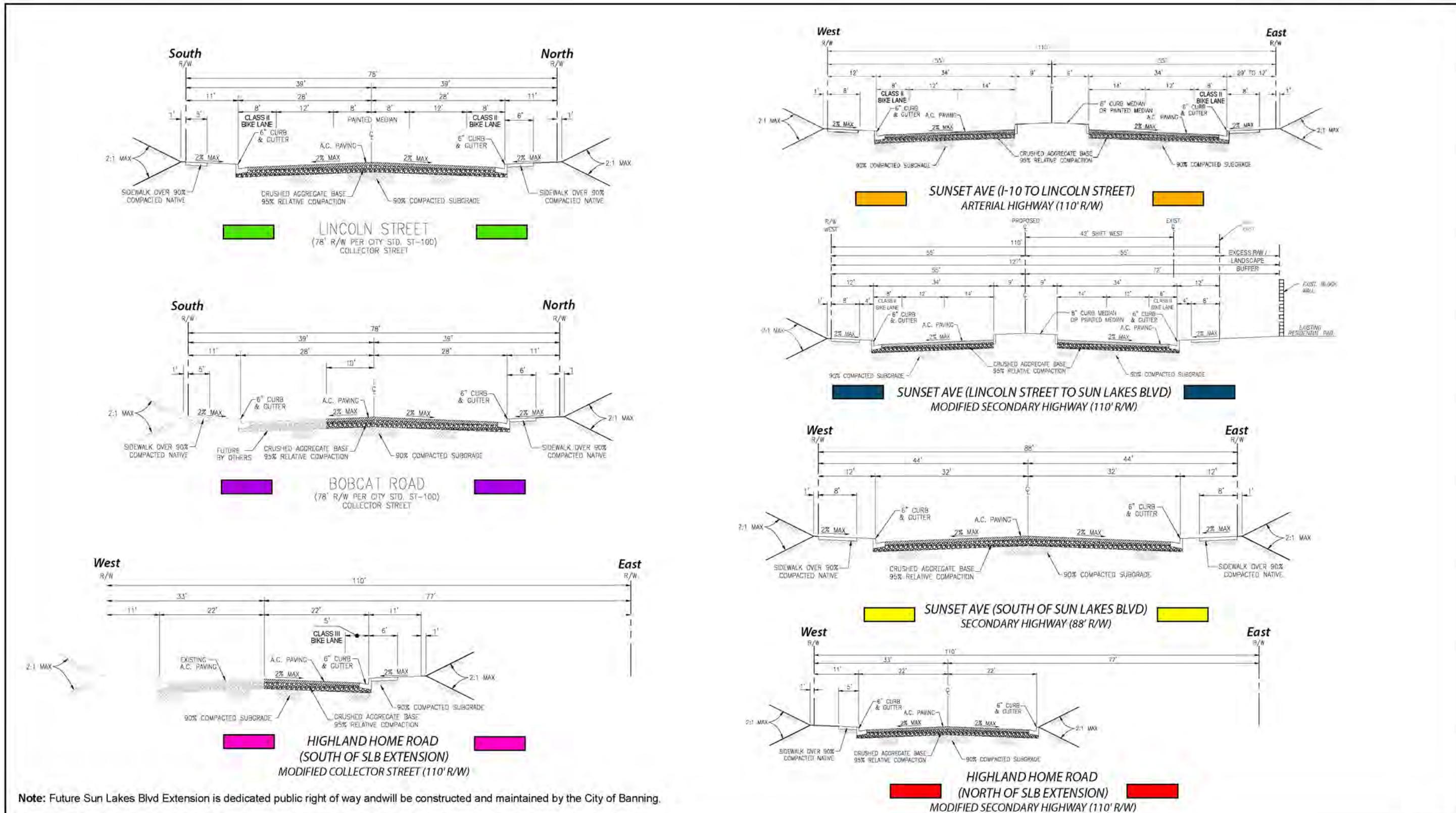
2.2.1. CIRCULATION IMPROVEMENT STANDARDS

1. The on-site system depicted on Figure 2-2, *Conceptual Circulation Plan*, shall serve as the preliminary roadway plan for implementing development.
2. Roadway alignments, designations, widths identified in the Specific Plan are subject to detailed engineering review and approval by the City Engineer and must comply with all conditions of approval of the Specific Plan and any Subdivision Map(s).
3. The Developer applying for an application submitted within this Specific Plan (Design Review or Conditional Use Permit) shall install traffic control measures per the recommendations of the project's Traffic Study or participate in the phased construction of traffic signals as determined by the City Engineer.
4. Driveways and access point locations, as shown in this Specific Plan, are conceptual until approved by the City Engineer (if necessary) and shall conform to the City's standard intersection spacing and access spacing, depending upon the street's classification.
5. Sight distances shall be reviewed by the City Engineer in conjunction with the preparation of grading, landscaping, and street improvement plans. This review shall ensure that setbacks allow for clear unobstructed sight distances at intersections.
6. Landscape requirements in roadway rights-of-way shall generally be in accordance with the Roadway Landscape Treatments as depicted in Chapter 4.
7. Landscaping within public road rights-of-way will require approval by the City of Banning and will be maintained by the Property Owner via a Property Owners' Association or similar mechanism.
8. All applicable Transportation Uniform Mitigation Fees (TUMF) shall be paid in accordance with Chapter 15.76 of the City of Banning Zoning Ordinance (Western Riverside County Transportation Uniform Mitigation Fee Program). The transfer of TUMF credits between Planning Areas and their respective owners is permitted by this Specific Plan.
9. Construction of the SLB Extension is a City Project, and the SUNSET CROSSROADS project is not required to construct the SLB Extension. The Developer of Sunset Crossroads may, at their own discretion, and subject to one or more agreements with City, construct all or a portion of the SLB Extension.
10. A detailed Traffic Impact Analysis shall be prepared for the project. Recommendations for intersection configurations, traffic control devices, or roadway segment and lane requirements within the property or adjacent frontage shall be the full responsibility of the project. However, fair share contributions shall be permissible for offsite improvements.
11. All mitigations identified in the project EIR/MMRP shall be complied with.
12. Street and utility infrastructure shall be installed such that future phases of work do not require trenching in new roadways. A pavement moratorium of 5 years will be implemented on all facilities accepted for maintenance by the City of Banning. If additional work for the project is required in existing roadways, the applicant will fully grind and overlay the entire roadway cross section.



Source(s): ESRI, Nearmap (2022), RCTLMA (2021) Proactive Engineering Consultants (10-19-2022)

FIGURE 2-2



Source(s): Proactive Engineering Consultants (11-16-2022, 05-01-2024)

FIGURE 2-3

2.3. POTABLE WATER PLAN

Adequate potable water service infrastructure is provided for SUNSET CROSSROADS using existing, planned and proposed potable water facilities. The Specific Plan is subject to the design requirements of the City of Banning Public Works Department and the demand criteria guidelines outlined in the City's Integrated Master Plan (IMP).

As shown on Figure 2-4, existing potable water lines within Pressure Zone 2721 exist in Sunset Avenue (24") and in the right-of-way of the future SLB Extension (18"). The proposed Potable Water Plan creates a looped system to serve the project site by constructing the following proposed improvements as depicted in *Figure 2-4, Conceptual Potable Water Plan*, and include the following improvements:

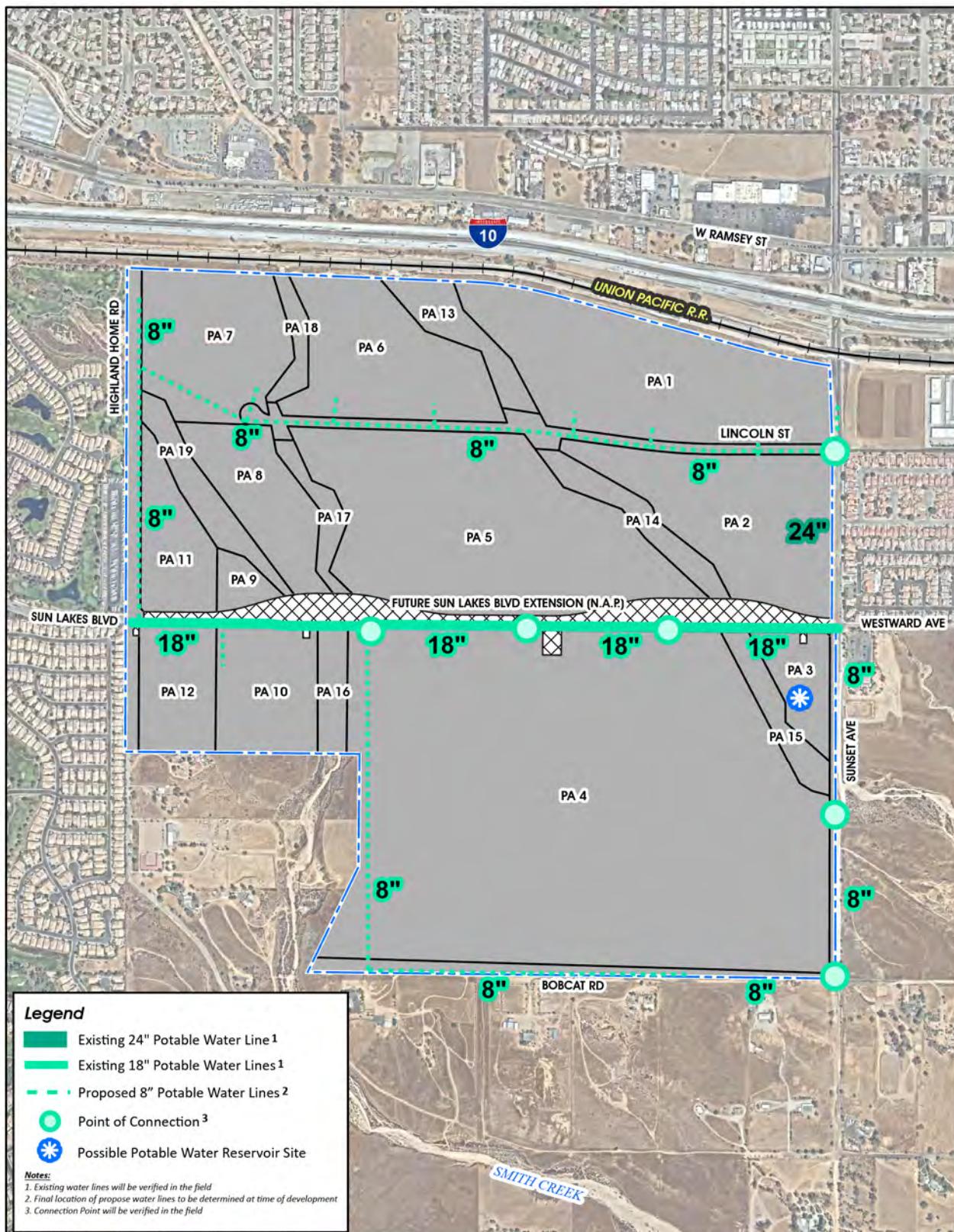
- ❖ A proposed 8" water line located in Lincoln Street and within Planning Area 7 to serve the Industrial and General Commercial uses located north of the SLB Extension. This proposed 8" water line connects to the existing 24" water line located in Sunset Avenue and to the proposed 8" water line in Highland Home Road.
- ❖ A proposed 8" water line located in Highland Home Road from the 18" water line located in the SLB Extension to the vicinity of the potential Electrical Substation in Planning Area 7, with a connection to the proposed 8" line in Lincoln, forming a looped system.
- ❖ A proposed 8" water line located in Bobcat Road with a point of connection to the proposed 8" line in Sunset Avenue.
- ❖ Proposed 8" water line located within Planning Area 4 to connect the existing 18" water line in the SLB Extension to the proposed 8" water line in Bobcat Road.

The locations, alignments, and sizes of facilities shown on Figure 2-4 are conceptual. Precise locations, alignments and sizes of potable water facilities will be determined at Specific Plan implementation. The locations, alignments, and sizes of facilities identified in this document may change, subject to the approval of City of Banning. Modifications to the alignments, locations, and sizes of facilities shown in the Specific Plan which are reviewed and approved by the City Engineer as part of the review and approval of an applications submitted within this Specific Plan, do not require modifications to the Specific Plan document.

2.3.1. POTABLE WATER IMPROVEMENT STANDARDS

- (1) All Potable Water lines shall be placed underground.
- (2) All Potable Water lines and facilities shall be designed in accordance with the requirements of the City of Banning.
- (3) The Specific Plan's Potable Water distribution system shall provide adequate flows for fire protection in accordance with requirements of the Riverside County Fire Department.
- (4) Proposed Potable Water facilities to be maintained by the City of Banning shall be placed within public utility easements or public rights-of-way, which shall include rights for the City of Banning.
- (5) The Potable Water infrastructure system shall be installed to the requirements of the City of Banning.
- (6) Construction of potable water lines should be linked to the construction of each phase of development. Installation of potable water lines should be linked to the construction of roadways in which the lines are located, and the need to service individual applications submitted within this Specific Plan as they come online.

(7) Applicant will provide to the City of Banning a potable water reservoir site within Planning Area 3 at a predetermined elevation. Site shall be provided to accommodate a 1.5 million gallon potable water reservoir, and reviewed by the City Engineer prior to dedication and acceptance.



Source(s): ESRI, Nearmap (2022), Proactive Engineering Consultants (01-2023)

FIGURE 2-4

2.4. RECYCLED WATER PLAN

The SUNSET CROSSROADS Specific Plan is served by a recycled water system consisting of existing, planned and proposed recycled water lines. The Specific Plan recycled water system is subject to the design requirements for the City of Banning Public Works Department and the demand criteria guidelines outlined in the City's Integrated Master Plan (IMP).

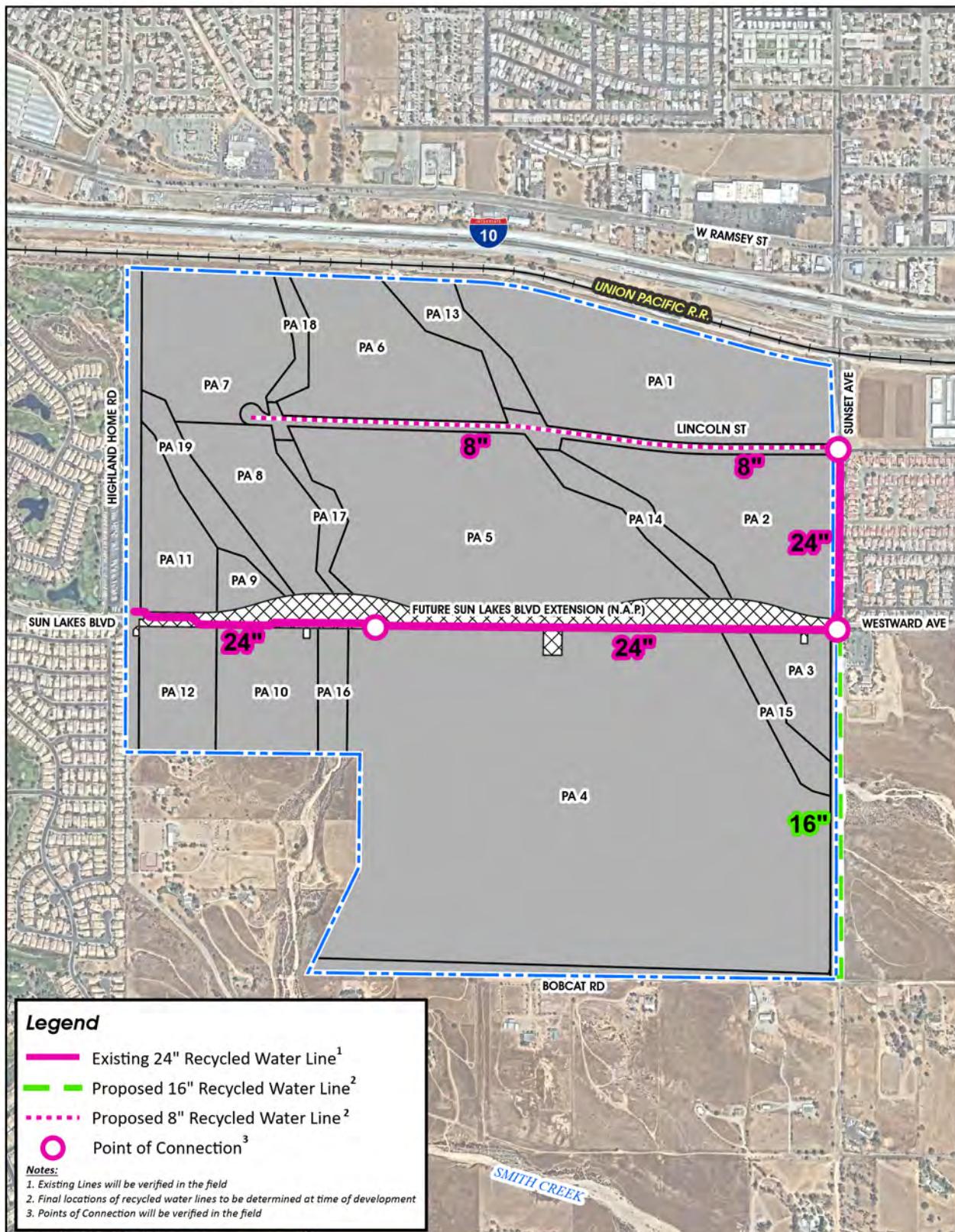
As depicted in Figure 2-5, a 24" recycled water line exists in Sunset Avenue from Lincoln Street to the SLB Extension and a 24" recycled water line exists within the SLB Extension easement area. Future non-potable Well R1 on Westward Avenue near the City's wastewater treatment facility plant will be the source for the SUNSET CROSSROADS project site, once completed. The well and its respective facilities are in design and under construction. The proposed recycled water facilities required to serve the Specific Plan are depicted in *Figure 2-5, Conceptual Recycled Water Plan* and include the following improvements:

- ❖ A proposed 8" recycled water line in Lincoln Street connects to the existing 24" recycled water line in Sunset Avenue and services the development north of the SLB Extension.
- ❖ A proposed 16" recycled water line in Sunset Avenue (between Bobcat and the SLB Extension) connects to the existing 24" line in the SLB Extension.
- ❖ Development south of the SLB Extension would connect to the existing 24" recycled water line in the SLB Extension or the proposed 16" line in Sunset Avenue.

The locations, alignments, and sizes of facilities shown on Figure 2-5 are conceptual. Precise locations, alignments and sizes of recycled water facilities will be determined at Specific Plan implementation. The locations, alignments and sizes of facilities identified in this document may change, subject to the approval of City of Banning. Modifications to the alignments, locations, and sizes of facilities shown in the Specific Plan which are reviewed and approved by the City Engineer as part of the review and approval of an application submitted within this Specific Plan, do not require modifications to the Specific Plan document.

2.4.1. RECYCLED WATER IMPROVEMENT STANDARDS

- (1) All Recycled Water lines shall be placed underground.
- (2) All Recycled Water lines and facilities shall be designed in accordance with the requirements of the City of Banning.
- (3) Proposed Recycled Water facilities to be maintained by the City of Banning shall be placed within public utility easements or public rights-of-way which shall include rights for the City of Banning.
- (4) The Recycled Water infrastructure system shall be installed to the requirements of the City of Banning.
- (5) Construction of recycled water lines shall be linked to the construction of each phase of development. Installation of recycled water lines shall be linked to the construction of roadways in which the lines are located, and the need to service individual applications submitted within this Specific Plan as they come online.



Source(s): ESRI, Nearmap (2022), Proactive Engineering Consultants (01-2023)

FIGURE 2-5

2.5. SEWER PLAN

The SUNSET CROSSROADS Specific Plan is served by a sewer system consisting of existing, planned and proposed sewer lines. The Specific Plan is subject to the design requirements for the City of Banning Public Works Department and the demand criteria guidelines outlined in the City's Integrated Master Plan (IMP).

As depicted on Figure 2-6, sewer lines exist in the SLB Extension (12") and along the westside Planning Areas 13 and 14 (15"). The existing 12" sewer line in the SLB Extension flows until it reaches the SLB Extension Lift Station, where wastewater is conveyed through an existing 10" force main toward the existing manhole at the intersection of Sunset Avenue and the SLB Extension. The proposed sewer system facilities required to serve the Specific Plan are depicted in Figure 2-6, Conceptual Sewer Plan and include the following improvements:

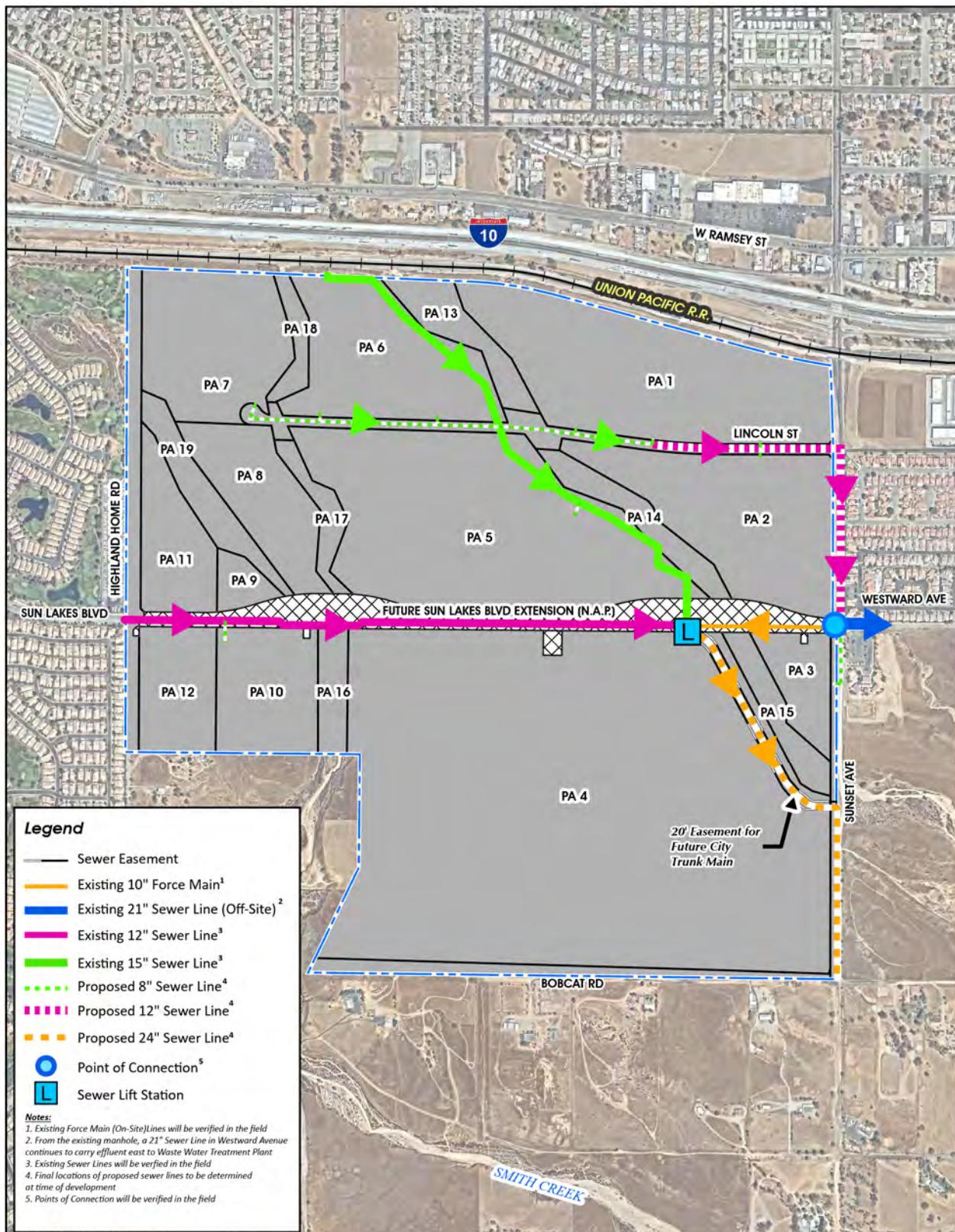
- ❖ A proposed 8" gravity sewer line connects to a proposed 12" gravity sewer line within Lincoln Street
- ❖ A proposed 12" gravity sewer line along Sunset Avenue from Lincoln Street to just south of the SLB Extension, connects to the existing sewer system at the existing manhole at the intersection of Sunset Avenue and the SLB Extension. There is no connection between this proposed line and the existing 15" line along Planning Areas 13 and 14.
- ❖ Development in the Planning Areas north of Lincoln Street, and Planning Areas 2 and 3 connect to the proposed 8" and 12" lines in Lincoln Street.
- ❖ Development in Planning Area 5 will connect to the existing 15" line along the western boundary of Planning Areas 13 and 14.
- ❖ Development in Planning Areas south of the SLB Extension will connect to the existing 12" line in the SLB Extension.

The locations, alignments, and sizes of facilities shown on Figure 2-6 are conceptual. Precise locations, alignments, and sizes of sewer facilities will be determined at Specific Plan implementation. The locations, alignments, and sizes of facilities identified in this document may change, subject to the approval of City of Banning. Modifications to the alignments, locations, and sizes of facilities shown in the Specific Plan which are reviewed and approved by the City Engineer as part of the review and approval of an application within this Specific Plan, do not require modifications to the Specific Plan document.

2.5.1. SEWER IMPROVEMENT STANDARDS

- (1) All sewer lines shall be placed underground.
- (2) All sewer lines and facilities shall be designed in accordance with the requirements of the City of Banning.
- (3) All new sewer facilities to be maintained by the City of Banning shall be placed within public utility easements or public rights-of way-which shall include rights for the City of Banning.
- (4) The sewer infrastructure system shall be installed to the requirements of the City of Banning.
- (5) Construction of sewer lines shall be linked to the construction of each phase of development. Installation of sewer lines shall be linked to the construction of roadways in which the lines are located, and the need to service individual application within this Specific Plan as they come online.
- (6) Prior to issuance of first occupancy permit, project shall demonstrate that adequate sewer capacity exists to serve the project.

(7) City of Banning master plans identify the future removal of the Westward Lift Station and replacement of that facility with a sewer trunk main through an easement in Planning Area 4 for the eventual connection to an off-site new sewer trunk main (by others). The project shall design and install a portion of the dry sewer trunk main to allow for this eventual conversion. If the off-site downstream trunk main is installed (by others) prior to the project being completed, the project shall abandon the lift station and connect to the new trunk main.



Source(s): ESRI, Nearmap (2022), Proactive Engineering Consultants (01-2023)

FIGURE 2-6

2.6. DRAINAGE AND WATER QUALITY PLAN

SUNSET CROSSROADS lies within the San Gorgonio Pass sub-basin in the northwest region of the Whitewater River Watershed. The SUNSET CROSSROADS Specific Plan is located within a grassland valley, with three natural drainage courses traversing the site. The project is subject to a local land use authority onsite retention standard (required by the Riverside County Flood Control & Water Conservation District per the Riverside County Watershed Protection Program for the Whitewater Region), which requires that the project retain 100% of the 100-year, 3-hour storm event. Planning Areas have been established in recognition of the natural drainage course in the eastern portion of the site in Planning Areas 13, 14, and 15 and the western drainage courses in Planning Areas 16, 17, 18, and 19 and have been designated for Open Space – Resource and restricted from development. The natural drainage courses flow northwest to southeast through the Specific Plan area. The 100-year flood zone continues along the entire flow path of the three existing drainage courses within the Specific Plan. No portion of the site is to be developed within a FEMA mapped Flood Plain.

The existing drainage courses include off-site areas north of I-10 which begin in the San Bernardino Mountains and flow south under Interstate 10 and the Union Pacific Railroad by an existing 8'x7' arch box culvert and existing 12'x10' arch box culvert, and outlets southerly to the existing drainage courses, where the SUNSET CROSSROADS Specific Plan is located. The western natural drainage course is comprised of two drainage channels, Smith Creek and Highland Wash, in the northern half of the project that confluence near the SLB Extension. The eastern natural drainage course, Pershing Creek, flows from the middle center of the northern boundary of the site. All natural drainage courses eventually confluence outside of the project limits, and these natural drainages continue to flow east until they confluence with the San Gorgonio River and ultimately join the Whitewater River.

As shown on Figure 2-7, *Conceptual Drainage and Water Quality Plan*, the proposed drainage system will consist of storm drainpipes, reinforced concrete pipes (RCP) ranging in size from 12" to 42", and fifteen (15) Water Quality Management Plan basins on-site:

- ❖ Two basins in Planning Area 2;
- ❖ One basin in Planning Area 3;
- ❖ Five basins in Planning Area 4;
- ❖ Three basins in Planning Area 5;
- ❖ One basin in Planning Area 6;
- ❖ One basin in Planning Area 7;
- ❖ One basin in Planning Area 8; and
- ❖ One basin in Planning Area 10

The drainage system routes the runoff from the proposed impervious surfaces to these proposed stormwater treatment and mitigation basins and each basin provides stormwater treatment and peak flow mitigation for their respective tributaries via RCP facilities.

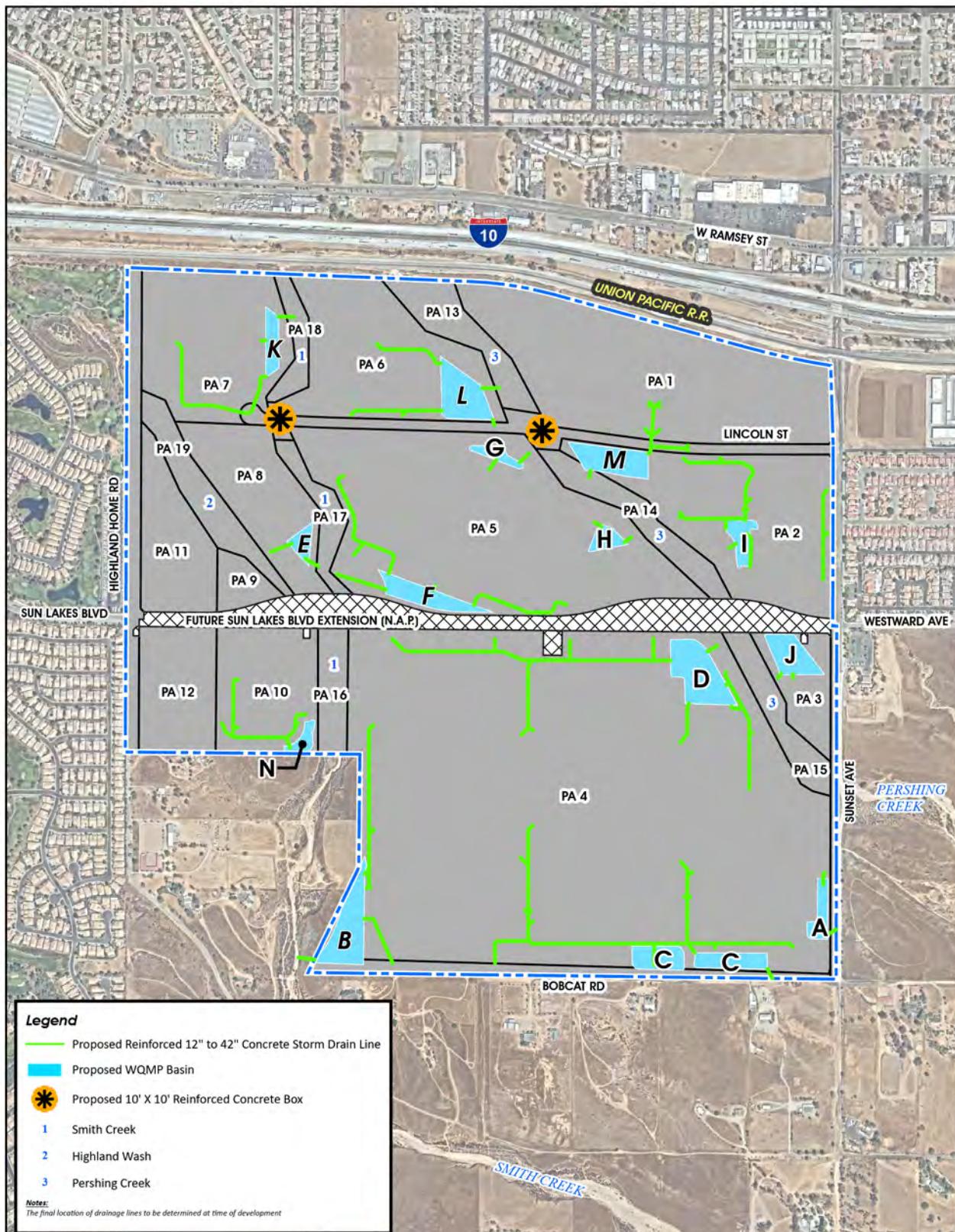
Two (2) 10' x 10' reinforced concrete box (RCB) facilities are proposed for the bridge crossings at Lincoln Street to preserve the existing natural drainage courses of the site.

The SUNSET CROSSROADS Specific Plan is subject to a local land use authority onsite retention requirement per the Riverside County Whitewater River Region Stormwater Quality Best Management Practice Design Handbook for Low Impact Development. The project shall implement LID principles per County standards throughout the site for storm water treatment and mitigation. Flood protection facilities will be designed in accordance with the requirements of the Riverside County Flood Control and Water Conservation District (RCFCWCD) and with adequate access easements and facilities provided. The location and size of facilities identified in this document may change, subject to the approval of the RCFCWCD.

The locations, alignments, and sizes of facilities shown in Figure 2-7 are conceptual. Precise locations, alignments, and sizes of drainage facilities will be determined at Specific Plan implementation. The locations, alignments, and sizes of facilities identified in this document may change, subject to the approval of City of Banning. Modifications to the alignments, locations, and sizes of facilities shown in the Specific Plan which are reviewed and approved by the City Engineer as part of the review and approval of an application within this Specific Plan do not require modifications to the Specific Plan document.

2.6.1. DRAINAGE AND WATER QUALITY IMPROVEMENT STANDARDS

- (1) All drainage and storm drain facilities shall be maintained by the City of Banning, Master Property Owners' Association, and/or other maintenance entity (pursuant to Table 5-1, *Maintenance Responsibilities*). All water quality and detention basins will be maintained by Master Property Owners' Association, Property Owners' Association, and/or Sub-Association.
- (2) All public storm drains greater than 36" in diameter and structures proposed for maintenance by Riverside County Flood Control and Water Conservation District (RCFCWCD) shall be designed and constructed to RCFCWCD standards. All plan sets related to any RCFCWCD facilities shall be reviewed, checked, and approved by said District prior the recordation of the applicable final map.
- (3) Drainage, storm drain, and flood control facilities and improvements shall be provided in accordance with City of Banning requirements to provide protection against flood hazard risks resulting from a 100-year storm event.
- (4) A National Pollutant Discharge Elimination System (NPDES) permit from the Regional Water Quality Control Board (RWQCB) shall be obtained prior to the commencement of construction and operational activities. The permits will require the Developer to implement source control and structural best management practices (BMPs) during and after construction activities. The City will be responsible for enforcing implementation of the BMPs.
- (5) This Specific Plan and future application submitted within this Specific Plan shall comply with the Water Quality Control Plan of the Regional Water Quality Control Board, Santa Ana Region.
- (6) Construction of drainage and storm drain facilities shall be linked to the construction of each phase of development. Installation of drainage and storm drain facilities shall be linked to the construction of roadways in which the lines are located, and the need to service individual applications submitted within this Specific Plan as they come online.
- (7) All wildlife and conservation agency permits are the responsibility of the project and shall be provided to the City of Banning prior to the issuance of any grading permit in each phase in which wildlife or agency permits are required.
- (8) Phased projects shall comply with drainage and water quality standards in effect at the time permits are requested.



Source(s): ESRI, Nearmap (2022), Proactive Engineering Consultants (01-2023)

FIGURE 2-7

2.7. GRADING PLAN

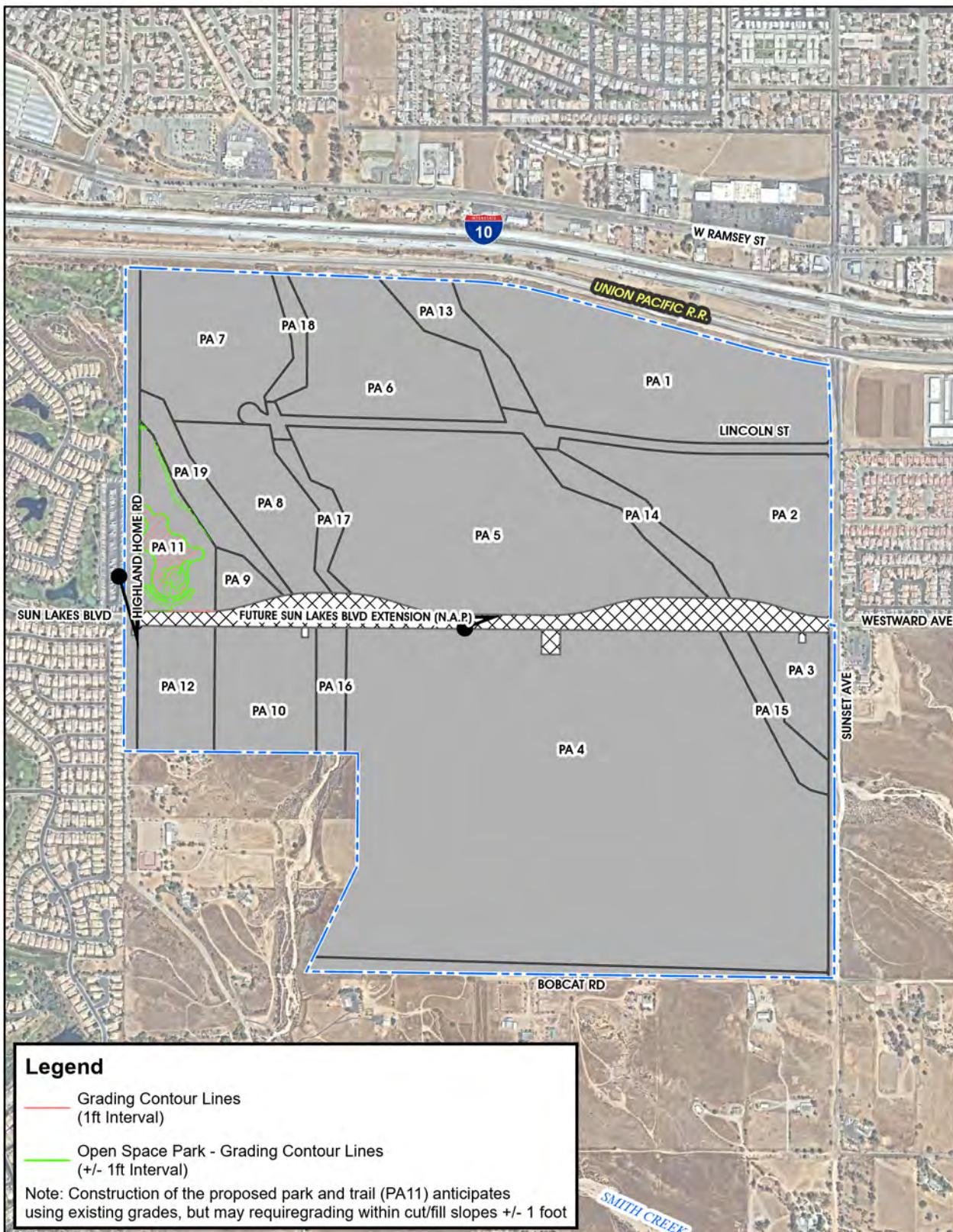
The SUNSET CROSSROADS Specific Plan is located within a grassland valley, with three natural drainage courses traversing the site in Planning Areas 13, 14, and 15 on the east and Planning Area 16, 17, 18, and 19 on the west. The conceptual grading design provides an overall balanced earthwork condition with approximately 2,266,112 cubic yards (CY) of cut and approximately 2,118,698 CY of fill. The grading of the Specific Plan is anticipated to occur in two (2) phases: 1) north of SLB Extension, and 2) south of SLB Extension. No import or exporting of earthwork is anticipated, however, earthwork may be transferred between the two (2) grading phases.

No project associated grading is proposed in Planning Areas 13 through 19, except for grading required for the construction of Lincoln Street. Other than passive open space areas, which may include trails, a Reverse Osmosis Facility Site, and/or a potential future fire station, no other grading or development is anticipated for Planning Area 12. All other Planning Areas will be graded to accommodate large generally flat pads suitable for building, park, and parking construction.

The proposed grading will result in elevations ranging from approximately 2,515 feet above mean sea level (MSL) in the northwest portion of the Specific Plan in Planning Area 7 to 2,415 feet above MSL at the bottom of a basin in the southwest portion of the Specific Plan in Planning Area 4. Grading construction will be done consistent with the recommendations of a Geotechnical Engineer and the City of Banning.

2.7.1. GRADING IMPROVEMENT STANDARDS

- (1) All grading activities shall conform to the City of Banning standards, and the California Building Code.
- (2) Grading of the Specific Plan may occur in one (1) phase, or in sub-phases, provided that adequate infrastructure and access is available to serve the Specific Plan.
- (3) Site grading, retaining wall design and construction shall be done consistent with the recommendations of a Geotechnical Engineer and the City of Banning Building and Safety Department.
- (4) Contour grading, slope rounding, and land forming shall be used, as appropriate and practical, to grade roadways and edge conditions into natural configurations consistent with the natural topography of the site.
- (5) A project-specific Master Grading Plan, and individual grading plans and stockpile/ borrow site plans for Planning Areas or phases shall govern such activity.
- (6) Prior to initial grading activities, a detailed soils report and geotechnical study for portions of the site anticipated to be graded shall be prepared to analyze on-site soil conditions and slope stability and will include appropriate measures to control erosion and dust during construction.
- (7) A grading permit shall be obtained from the City of Banning, as required by City Ordinance No. 18.01.040, prior to grading.
- (8) Pursuant to the requirements of the State Water Resources Control Board, a state-wide general National Pollution Discharge Elimination System (NPDES) construction permit will apply to all project construction activities involving clearing, grading or excavation that disturbs at least 5 acres or activity which is part of a larger common plan of development of five (5) acres or greater.
- (9) Applicant shall provide permits to the City from all affected wildlife and conservation agencies prior to the issuance of any grading permit in each phase in which wildlife or agency permits are required.
- (10) All grading shall comply with mitigation measures identified in the EIR/MMRP.

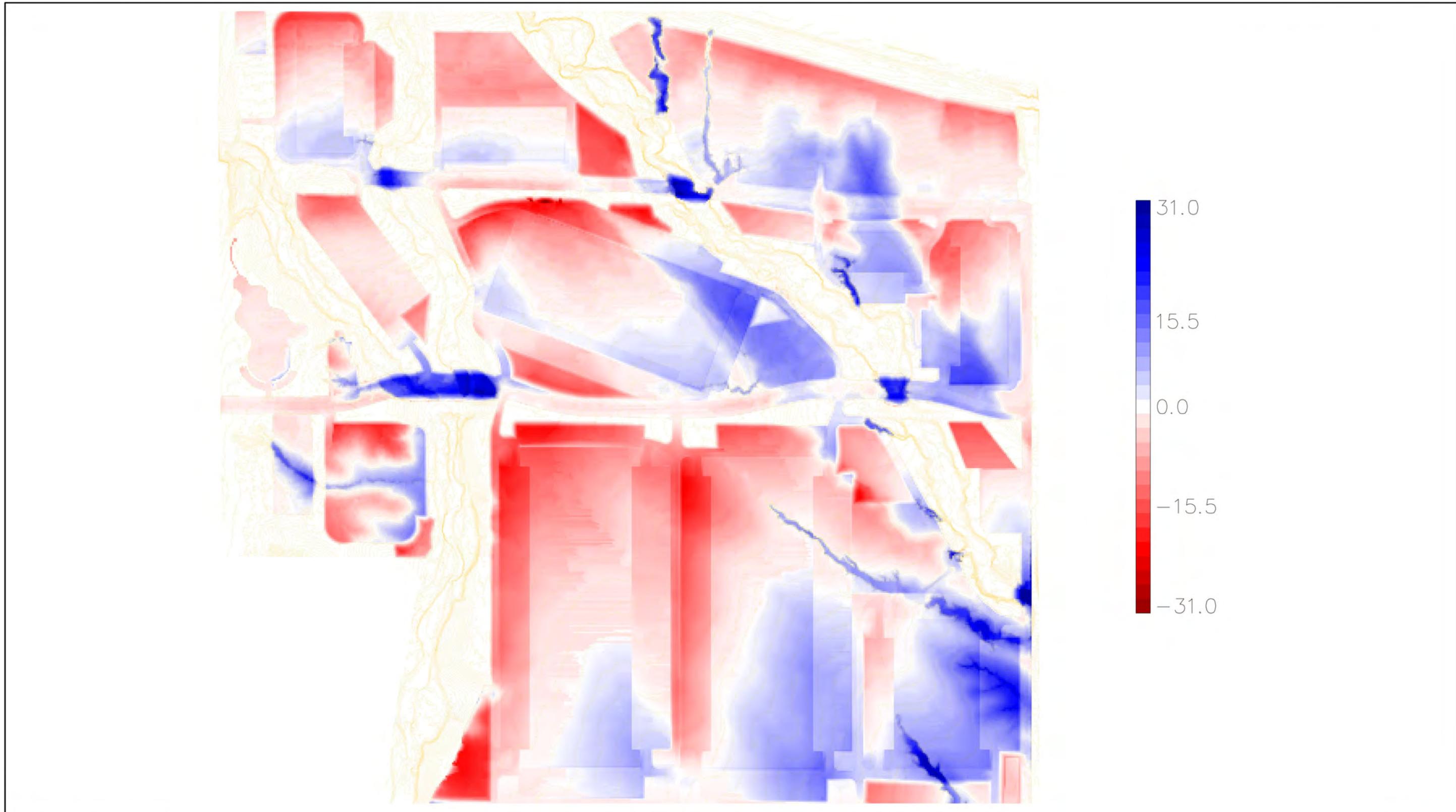


Source(s): ESRI, Nearmap (2022), Proactive Engineering Consultants (January 2023)

FIGURE 2-8



0 275 550 1,100
[REDACTED]
[REDACTED] Feet



Source(s): Proactive Engineering Consultants (07-27-2022)

FIGURE 2-9

2.8. OPEN SPACE PLAN

A central element of the SUNSET CROSSROADS Specific Plan is the approximately 12.6 acres of Open Space – Parks and 53.0 acres of Open Space - Resource (see *Figure 2-1, Land Use Plan*). Approximately 53.0 acres (Planning Areas 12 through 19) are designated as Open Space-Resource in order to preserve the three existing natural drainage courses. No grading is proposed in Planning Areas 13 through 19, except for grading required for the construction of Lincoln Street. Planning Area 12 provides an open space buffer between the Specific Plan and the existing Sun Lakes community to the west and includes open space areas which may include trails, as well as locations for a Reverse Osmosis Facility Site and a future Fire Station site.



Approximately 12.6 acres in Planning Area 11 are designated as Open Space-Parks to offer the City of Banning and its residents a 5.0-acre public passive park which may include a tot lot, playground, picnic tables, and trail system that will connect to trails in the remaining 7.6 acres of open space, providing an open space buffer between the industrial uses and the existing Sun Lakes community to the west.



Passive recreational opportunities are provided to employees and visitors of SUNSET CROSSROADS through curb-adjacent sidewalks, pedestrian paths, and trails within Planning Areas 11 and 12. These amenities encourage and enhance pedestrian activity throughout SUNSET CROSSROADS. Provisions for sidewalks and pedestrian walkways, bicycle parking facilities, and employee and visitor gathering areas interior to the planning areas are set forth in Chapter 4, *Design Guidelines*.

In addition, to encourage social interaction, the Industrial and General Commercial building sites within SUNSET CROSSROADS may include outdoor employee break areas with tables affixed to the ground to provide employees with a location to eat, gather, and enjoy being outside. Shading of these areas may be achieved through a combination

of shade trees, umbrellas, or man-made shade structures. Other recreational amenities within the Industrial and General Commercial areas may include, but are not limited to, pedestrian walkways, seating areas, overhead structures, and open space areas. No Class I or Class II Bike Lanes are anticipated for the Specific Plan, however, Class III Bike Lanes which are unstriped may be utilized by riders along public roadways.

2.8.1. OPEN SPACE IMPROVEMENT STANDARDS

1. Gathering areas, and landscaping within private roads and driveways will be the responsibility of a Master Property Owners' Association, Property Owners' Association, or Sub-Association.
2. The 5.0-acre public passive park and landscaping within public rights-of-way will be the responsibility of the City of Banning.
3. All gathering areas will be landscaped and, where necessary, irrigated in a manner that is conducive to the type of plant material and landscape setting.
4. Landscaping within gathering areas shall be further governed by Chapter 4, *Design Guidelines*, of this Specific Plan.
5. Landscaping within gathering areas and park areas shall conform to the standards set forth by the City of Banning.

2.9. FIRE PROTECTION PLAN

C A Fire Protection Plan (FPP) dated July 2022 has been prepared by Dudek, evaluates and identifies the potential fire risk associated with the Project's land uses and identifies requirements for water supply, fuel modification and defensible space, access, building ignition and fire resistance, and fire protection systems, among other pertinent fire protection criteria, including a proposed development plan for the Project site described below. The purpose of this FPP is to generate and memorialize the fire safety requirements and standards of the Riverside County Fire Department (RCFD), which is the contracted fire department for the City of Banning, along with Project-specific measures based on the Project site, its intended use, and its fire environment, including both the proposed development plan and any modifications to buildings permitted pursuant to the Specific Plan. This section summarizes portions of the FPP prepared by Dudek, which should be consulted for the full analysis.

While the Specific Plan is located in a wildland-urban interface (WUI) setting, it is not located in an area statutorily designated as a Moderate, High or Very High Fire Hazard Severity Zone (FHSZ) within a Local Responsibility Area (LRA) or State Responsibility Area (SRA) by the California Department of Forestry and Fire Protection (CAL FIRE FRAP 2008) or Riverside County; rather, the Project site is designated as Non-VHFHSZ.² As the Project is not located within an area statutorily mapped as a fire hazard severity zone, the Project is not required to adhere to the ignition resistant construction requirements of California Building Code, Chapter 7A. However, the construction type for this Project would be substantially compliant with Chapter 7A including the building roofs, which will not include the Class A system of materials, but will comply with section 1505.1.2 as roof assemblies will be Class C at a minimum as required. Since the roof material assembly will meet the Class C non-combustible assembly requirements, it meets code and is also consistent with Chapter 7A of the Building Code.

The developed portion of the Project site is proposed for improvements that include construction of up to 5,545,000 square feet of industrial development on approximately 392.0 acres and up to 268,400 square feet of commercial uses, including Travel Center retail uses, a gasoline and diesel fueling facility and a hotel with 125 rooms comprised of approximately 90,000 square feet in an approximately 47.9-acre area on the Project site. Although the Project site is not designated as a FHSZ, due to its proximity to areas identified as Wildland Urban Interface (WUI), the entire Project site has been designed with fire protection as a key objective. The Project site improvements are designed to facilitate emergency apparatus and personnel access throughout the Project site. Driveway and road improvements with fire apparatus turnarounds provide access to the sides of every building. Water availability and flow will be consistent with requirements including fire flow and hydrant distribution required by local and state codes. These features along with the ignition resistance of all buildings, the interior sprinklers, and the pre-planning, training and awareness will assist responding firefighters through prevention, protection, and suppression capabilities.

As determined during the analysis of the Project site and its fire environment, in its current condition, the Project site may include characteristics that, under favorable weather conditions, could have the potential to facilitate fire spread. Under extreme conditions, wind-driven wildfires from nearby undeveloped land could cast embers onto the property. Once the Project is built, the on-site fire potential will be much lower than its current condition due to conversion of fire facilitating wildland fuels to ignition resistant buildings, parking areas, managed landscapes, fuel modification areas, improved accessibility for fire personnel, and structures built to the latest ignition and ember resistant fire codes.

As detailed in the FPP, the fire protection systems recommended for the Project site will include a redundant layering of protection methods that have proven to reduce overall fire risk. The requirements and recommendations included

² Per the CAL FIRE FRAP map for Banning, the portion of the Project site north of Westward Rd is designated as Non-VHFHSZ in a LRA (adjacent lands north, northeast and west of the Project site share this designation). The southern portion of the Project site is in an SRA; however, is not designated as a FHSZ (adjacent lands east and south are designated as High and Very High FHSZ in an SRA). Per Riverside County GIS, the entire site is neither Moderate, High nor Very High FHSZ.

herein are performance based and site-specific, considering the Project's unique characteristics rather than a prescriptive, one-size-fits-all approach. The fire protection systems are designed to increase occupant and building safety, reduce the fire risk on site, to minimize risks associated with typical uses, and aid the responding firefighters during an emergency. No singular measure is intended to be relied upon for the Project site's fire protection, but rather, a system of fire protection measures, methods, and features combine to result in enhanced fire safety, reduced fire potential, and improved safety in the development.

Early evacuation for any type of wildfire emergency at the Project is the preferred method of providing for occupant and business safety, consistent with the Owner's and the RCFD current approach for evacuation. As such, the Project's Owner and Property Management Company will formally adopt, practice, and implement a "Ready, Set, Go!" (Riverside County Fire Department 2020) approach to site evacuation. The "Ready, Set, Go!" concept is widely known and encouraged by the State of California and most fire agencies, including; Pre-planning for emergencies, including wildfire emergencies, focuses on being prepared, having a well-defined plan, minimizing potential for errors, maintaining the site's fire protection systems, and implementing a conservative (evacuate as early as possible) approach to evacuation and site uses during periods of fire weather extremes.

2.9.1. FIRE PROTECTION IMPROVEMENT STANDARDS

The following Fire Protection Plan Improvement Standards and implementation measures are based on the analysis and findings of the Dudek Fire Protection Plan dated July, 200 and shall be provided as part of development plan for each application submitted within this Specific Plan:

1. All development within the SUNSET CROSSROADS Specific Plan shall be constructed consistent with the applicable portions of the Fire Protection Plan (FPP) dated July 2022 prepared by Dudek.
2. Project buildings to be constructed of ignition resistant¹ construction materials common for this type of concrete product and to include automatic fire sprinkler systems based on the latest adopted Building and Fire Codes for occupancy types.
3. Fuel Modification will be provided around the perimeter of the on-site structures, and should typically be 100 feet wide, though there are a few areas where 100-feet is not achievable as detailed herein. Additionally, an extended fuel modification width will be provided around many structures due to the hardscape landscape design.
4. For any Planning Area in which the square footage or footprint of a proposed building has been modified from that described in this fire protection plan, the applicant shall submit and the RCFD shall have approved the revised fire protection plan for the Planning Area, consistent with Item 9, below.
5. Landscape plantings should not utilize plants listed on Appendix E of the FPP dated July 2022 (prohibited plants) which are prohibited because they have been found to be highly flammable.
6. Fire apparatus access roads (i.e., public and private streets) will be provided throughout the development and will vary in width and configuration, but will all provide at least the minimum required unobstructed travel lanes, lengths, turnouts, turnarounds, and clearances required by applicable codes. Primary access and internal circulation will comply with the requirements of the RCFD.
7. Buildings will be equipped with automatic commercial fire sprinkler systems meeting RCFD requirements.
8. The Project shall demonstrate provision of water capacity and delivery for a reliable water source for operations and during emergencies requiring extended fire flow.
9. Should future iterations of the Project's site plan result in buildings that do not achieve a minimum of 100 feet of defensible space, then alternative materials and methods may be proposed to provide the functional equivalency of a full 100 feet of defensible space. Alternative materials and methods will be to the satisfaction of the RCFD and may include structural hardening enhancements or landscape features, like non-combustible walls. The following measures shall be established in the CC&Rs for the Project and implemented by the Property Management Company. Annual maintenance should occur before May 1st of each year and inspected by RCFD or an approved third party.
10. On-going maintenance of all fuel modification will be managed by Owner's, Property Management Company, or another approved entity, at least annually or as needed. 10. The Property Owner's or Property Management Company, will provide business owners informational brochures at time of occupancy, which should include an outreach and educational role to ensure fire safety measures detailed in the Dudek FPP dated July 2022, have been implemented and prepare development-wide "Ready, Set, Go!" plans.

DEVELOPMENT STANDARDS

3

CHAPTER THREE establishes the Specific Plan's Implementation Regulations, comprised of the General Development Criteria, Permitted, Conditional, Ancillary, and Prohibited Uses, Development Standards, and Planning Area Standards.



- 3.1 GENERAL DEVELOPMENT CRITERIA
- 3.2 PERMITTED, CONDITIONALLY PERMITTED, ANCILLARY AND PROHIBITED USES
- 3.3 GENERAL COMMERCIAL DEVELOPMENT STANDARDS
- 3.4 INDUSTRIAL DEVELOPMENT STANDARDS
- 3.5 PLANNING AREA STANDARDS

CHAPTER 3 | DEVELOPMENT STANDARDS

This section establishes certain of the Implementation Regulations for each Planning Area in the SUNSET CROSSROADS Specific Plan. The Implementation Regulations provided herein work in concert with the architecture and landscape design guidelines set forth in Chapter 4, *Design Guidelines*, to achieve the desired vision for SUNSET CROSSROADS. This chapter shall apply to all Subdivision Maps, Design Reviews, Conditional Use Permits, and any other action requiring administrative or discretionary approval for implementation of the SUNSET CROSSROADS Specific Plan.

Section 3.1 provides General Development Criteria for the entire Specific Plan area to ensure the orderly, efficient and coordinated development of the SUNSET CROSSROADS SPECIFIC PLAN. *Table 3-1 Permitted Uses* establishes the land uses and review procedures consistent with the vision, goals, and intent, for the General Commercial, Industrial and Open Space land uses within the Specific Plan. The Development Standards presented in Sections 3.3 and 3.4 and included in Tables 3-2 and Table 3-3, identify the criteria affecting the development of the General Commercial and Industrial Planning Areas, respectively. The Planning Area Standards in Section 3.5 for each Planning Area reference the relevant Figures that depict desired architecture and landscaping elements contained throughout this Specific Plan for each Planning Area. The Planning Area Figures (Figures 3-1 through 3-6) presented in this section are derived from the Specific Plan Land Use Plan for SUNSET CROSSROADS (Figure 2-1, *Land Use Plan*).

Planning Areas within the SUNSET CROSSROADS Specific Plan were formed on the basis of logical separate areas of land use and development to ensure that implementation of the Industrial, General Commercial, and Open Space land uses are consistent with the quality of development and vision of City of Banning for SUNSET CROSSROADS.

3.1. GENERAL DEVELOPMENT CRITERIA

To ensure the orderly and sensitive development of the SUNSET CROSSROADS Specific Plan area, the following General Development Criteria apply to the entire Specific Plan area and augment the more location-specific Development Standards and Planning Area Standards provided elsewhere in Chapter 3. The Specific Plan-wide General Development Criteria are as follows:

1. The SUNSET CROSSROADS Specific Plan shall be developed as an employment and retail entertainment center containing General Commercial, Industrial, Open Space - Parks, and Open Space - Resource land uses on approximately 533.8 acres, as illustrated on Figure 2-1, *Land Use Plan*.
2. The SUNSET CROSSROADS Specific Plan shall be developed with a maximum of 5,813,400 square feet of building area, in addition to 125-room limited-service hotel (approximately 90,000 square feet). The 5,813,400 square feet of building area consists of a Maximum Building Square Footage of 268,400 sf of building area in Planning Area 1 and a Maximum Building Square Footage of 5,545,000 SF in Planning Areas 2-10. The approximately 90,000 square feet of hotel use is not counted as part of the General Commercial's 268,400 Maximum Building Square Footage because traffic analysis for hotels is calculated by number of rooms, not square footage.
3. Except as modified by this Specific Plan or as approved by the Community Development Director pursuant to Chapter 5, *Implementation Plan*, development of the property shall be in accordance with the mandatory requirements of City of Banning ordinances.

4. Planning Area net acreage (and net acreage covered by a given land use designation) may be expanded or reduced without a Specific Plan Amendment, through the Minor Modification process pursuant to Section 5.2.3, provided the modification does not increase or decrease the size of any Planning Area by more than 20%.
5. Development intensity/density or building square footage may be increased by no more than 20% within any or all of the Planning Areas without a Specific Plan Amendment, through the Minor Modification process pursuant to Section 5.2.3, provided that the overall Maximum Building Square Footage for Industrial uses and for General Commercial uses within this Specific Plan is not exceeded.
6. Development intensity/density or building square footage may be reduced in any or all of the Planning Areas without a Minor Modification or Specific Plan Amendment.
7. The permitted building intensity/density or building square footage within any Planning Area pursuant to an approved application, may be increased by a subsequent application submitted within this Specific Plan for the same Planning Area, through the Minor Modification process pursuant to Section 5.2.3, provided that such increase does not result in the Maximum Building Square Footage permitted by the Specific Plan document being exceed for Industrial uses or for General Commercial uses, nor exceed the maximum of 125 hotel rooms.
8. The City of Banning Planning Department shall prepare and maintain a Development Intensity Table which identifies each approved application submitted within this Specific Plan by Planning Area along with the building square footage approved and/or constructed on that Planning Area pursuant to an approved application, so that a running total of the approved square footage is monitored to assure that the Maximum Building Square Footage permitted by the Specific Plan document (in either the Industrial or General Commercial) is not exceeded.
9. Buildings may be constructed across Planning Area boundaries with the same Land Use Designation, which cross over into abutting Planning Areas, subject to all of the applicable Implementation Regulations and Design Guidelines of this Specific Plan. Buildings constructed across Planning Area boundaries or lot lines shall not, by themselves, trigger the requirement for a Minor Modification or Specific Plan Amendment.
10. Prior to issuance of a building permit for construction of any new use contemplated by this Specific Plan approval, the applicant shall first obtain clearance from the Planning Department verifying that all pertinent conditions of this Specific Plan approval have been satisfied for the phase of development in question.
11. A review in compliance with the California Environmental Quality Act (CEQA) shall be conducted to determine potential environmental impacts resulting from each Subdivision Map, Zone Change, Specific Plan Amendment, or other discretionary land use application required to implement this Specific Plan, unless such proposal is determined by the Community Development Director to be ministerial, consistent with the project EIR and to not require subsequent environmental review, or is exempt from the provisions of CEQA. Any required CEQA review shall be prepared as part of the review process for the application submitted within this Specific Plan.
12. Lots or parcels created within the Specific Plan area pursuant to any concurrent or subsequent tentative subdivision map(s) shall be consistent with the Implementation Regulations of the Specific Plan herein applied to the site, or as modified pursuant to Chapter 5, Implementation Plan.
13. For the security and safety of future employees and visitors, the applicant and/or Developer shall incorporate the following design concepts within each individual Design Review or Conditional Use Permit Application, or as approved by the Community Development Director:
 - a. Circulation for pedestrians, vehicles, and police patrols.
 - b. Lighting of streets, parking areas, and walkways.
 - c. Visibility of doors and windows from the street and between buildings, where practical.

- d. Fencing heights and materials (which are Developer's responsibility).

15. All lighting within this Specific Plan shall adhere to the following standards:

- a. Lighting shall be limited to that necessary to light the project site. No lighting source shall be visible, or shall be permitted to spill over to adjacent properties.
- b. Lighting shall not be permitted which blinks, flashes, or is of unusually high intensity or brightness. All lighting fixtures shall not have a visible light source and must be shielded and directed downward and away from adjoining properties and public rights-of-way.
- c. Lighting in commercial and industrial projects should be only the minimum required for safety and security.
- d. Light standards should be limited to eighteen to thirty-four feet.
- e. Lighting should be integrated into the structure's architecture to the greatest extent possible.
- f. All lighting requirements in Section 17.24.100 of the City of Banning Zoning Ordinance are superseded by these standards.

16. Off street parking areas shall be regulated by these standards and other regulations within the City Zoning Ordinance are superseded:

- a. Access.
 - 1. All parking areas shall provide suitable maneuvering room so that all vehicles may enter an abutting street in a forward direction.
 - 2. No parking space shall be located so that a vehicle will maneuver within 25 feet of a parking entrance/vehicle entrance/parking aisle, vehicular entrance measured from the face of the curb.
- b. Dimensional Requirements. Dimensional requirements for off-street parking include the following:
 - 1. Parking stalls shall be non-perpendicular to the parking aisle whenever possible.
 - 2. Parking structures may be subject to dimensional adjustment, but in no case shall the stall width be less than eight feet and six inches. Reductions in design standards shall be subject to approval by the City Engineer and shall be discouraged.
- c. Drainage. Off-street parking areas shall be so designed that surface water will not drain over any sidewalk, or adjacent property.
- d. Driveways.
 - 1. Commercial/ Industrial
 - a. Access standards shall be maintained by the Public Works Department.
 - b. Drive aisles shall be a minimum width of 15 feet for a one-way driveway, and 24 feet for a two-way driveway.
- e. Landscaping, Screening and Shading. A minimum of 15 percent of the net area of all parking areas shall be landscaped as follows:

1. Where parking areas adjoin a public right of way, a landscaped planting strip equal to the required yard setback shall be established and continuously maintained between the public right of way and parking area.
2. Any planting, sign, or any other structure within safety sight-distance of a driveway shall not exceed 30 inches in height.
3. Pedestrian access shall be provided throughout the landscaped areas.
4. At least one 24 inch box tree for every four spaces shall be included in the development of the overall landscape program. The maximum spacing between trees in parking areas shall be 30 feet; however, appropriate clustering of trees may be permitted. Landscaping islands are required at both ends of all parking rows.
5. All areas in a parking lot not used for driveways, maneuvering areas, parking spaces or walks, shall be permanently landscaped with suitable materials and permanently maintained.
6. A concrete curb, six inches high and six inches wide, shall abut all parking areas.
7. All landscaped areas shall be a minimum interior dimension of six feet in width.
8. Permanent and automatic irrigation systems shall be installed and permanently maintained in all landscaped areas, in conformance with the City's water conservation regulations in Chapter 17.32.
9. To increase the parking lot landscaped area, a maximum of two feet of the parking stall depth may be landscaped in lieu of asphalt while maintaining the required parking dimensions. This overhang is in addition to the required yard setbacks.
10. The landscaping plan shall provide for a variety of plant materials with an emphasis on drought tolerant species, and shall include a legend showing common names and sizes.
11. For screening purposes, all commercial, industrial and public parking areas abutting residentially designated property shall have a six foot high solid architecturally treated decorative masonry wall approved by the Community Development Director. All wall treatments shall occur on both sides.
12. Although any reasonable combination of shading methods can be utilized, all parking areas must provide at least 30 percent permanent shading for parked vehicles within two years of planting.
13. If trees are used, they may not thereafter be trimmed in a way which reduces the effectiveness of their shading ability.

f. Security and Lighting.

1. All parking facilities shall be designed, constructed and maintained with security as a priority to protect the safety of the users.
2. Adequate illumination for security and safety shall be provided in all parking areas. Lighting shall be energy efficient and in scale with the height and use of the structure. Any illumination, including security lighting, shall be shielded, visibility of light source eliminated and directed away from adjoining properties and public rights of way.

g. Location of Required Parking Spaces.

1. All parking spaces shall be located on the same parcel as the structure or the use unless otherwise approved by the review authority.

h. Maintenance. All required parking facilities shall be permanently maintained, free of litter and debris.

- i. **Parking Structures.** All parking structures shall be landscaped as follows:
 1. All landscaping shall be permanently maintained and automatically irrigated. The parking structure shall have continuous minimum ten foot perimeter landscaping with vertical elements, such as trees or climbing vines at least every 20 feet.
 2. Entries and exits of the parking structure shall include a minimum six-foot wide landscaped median island and accent paving in the driveway.
 3. Landscaped materials, excluding vertical element openings, shall be provided in planters and/or pots for at least five percent of the total surface deck area. The planters and pots shall be distributed through the top deck area, and the perimeter of intermediate decks.
 4. Lighting shall not spill beyond the surface deck, and shall not spill onto other properties. Lighting fixtures shall not exceed four feet in height.
- j. **Shared Parking.**
 1. Parking facilities may be shared if multiple uses cooperatively establish and operate the facilities and if these uses generate parking demands primarily during hours when the remaining uses are not in operation.
 2. The applicant shall have the burden of proof for a reduction in the total number of required off-street parking spaces, and written documentation shall be submitted substantiating their reasons for the requested parking reduction. Shared parking may only be approved if:
 - a. A sufficient number of spaces are provided to meet the greater parking demand of the participating uses;
 - b. Satisfactory evidence, as determined by the Community Development Director, has been submitted by the parties operating the shared parking facility, describing the nature of the uses and times when the uses operate so as to demonstrate the lack of potential conflict between them; and
 - c. Any additional covenants, deed restrictions or other agreements or documents as may be deemed necessary by the Community Development Director, are executed to assure that the required parking spaces provided are maintained, and that uses with similar hours and parking requirements, remain for the life of the commercial/industrial development.
- k. **Slope.**
 1. Driveways shall have no grades exceeding an eight percent slope, unless approved by the City Engineer.
 2. Parking areas shall be designed and improved with grades not to exceed a five percent slope.
- l. **Striping and Surfacing**
 1. All parking spaces shall be striped in accordance with City requirements. The striping shall be maintained in a clear and visible manner. Each exit from any parking area shall be clearly marked with a "STOP" sign.
 2. Driveway and parking areas should be surfaced with a minimum thickness of three inches of concrete, asphaltic concrete, or a material approved by the City Engineer with bituminous surfacing over a minimum thickness of four inches of an aggregate base material.

3. For areas which are close to trees and shrubs, a porous surface may be used such as gravel, if this will aid in bringing rainwater to the roots of the trees, and if this is approved by the City Engineer.
- m. Curbing and Wheel Stops.
 4. Continuous concrete curbing at least six inches high and six inches wide shall be provided at least three feet from any wall, fence, property line, walkway or structure where parking and or drive aisles are located adjacent thereto. Curbing may be left out at structure access points.
 5. The space between the curb and wall, fence, property line, walkway or structure shall be landscaped. The clear width of a walkway adjacent to overhanging parked cars shall be four feet.
 6. All parking lots shall have a continuous curbing at least six inches high and six inches wide around all parking areas and aisle planters. Wheel stops shall not be used in lieu of curbing to protect landscaping, signage structures and walls.
- n. Parking Adjustment. Concurrent with an application for Design Review and/or Conditional Use Permit and application for a parking adjustment may be approved by the review authority subject to the findings:

Findings for a Parking Adjustment

1. The zoning regulations applicable to the property do not allow a reasonable use comparable to similar developments in the same zoning district;
2. The hardship for which the variance is requested is unique to the property area;
3. The variance will not alter the character of the area adjacent to the property, will not impair the use of adjacent conforming property, and will not impair the purpose of the regulations of the zoning district in which the property is located;
4. Neither present nor anticipated future traffic volumes generated by the use of the site or the sites in the vicinity require strict or literal interpretation and enforcement of the specific regulation;
5. The granting of this parking variance will not result in the parking or loading of vehicles in public streets in such a manner as to interfere with the free flow of street traffic;
6. The granting of this variance will not create a safety hazard or any other condition inconsistent with the objectives of this Ordinance; and
7. The variance will run with the use or uses to which it pertains and shall not run with the site.

17. All landscaping shall be designed, installed and maintained in a manner consistent with the City of Banning Municipal Ordinance Section 8.28.170 and Chapter 17.32 (Water Efficient Landscape Ordinance) where not superseded by this Specific Plan.

18. Prior to the issuance of the first occupancy permit within the project, the Sign Program and Signage Design Guidelines shall be prepared and approved by the City of Banning. The Sign Program and Signage Design Guidelines shall provide adequate and appropriate project, street, building, tenant identification, pedestrian path, and wayfinding signage for the anticipated variety of building sizes, designs, and uses that shall be consistent with this Specific Plan.

19. Common areas identified in the Specific Plan are intended to be owned and maintained pursuant to Table 5-1, Maintenance Responsibilities, as described below:

- a. One or more permanent master maintenance organizations shall be established for the Specific Plan area, such as a Master Property Owners' Association, Property Owners' Association, and/or Sub-Association, to assume ownership and/or maintenance responsibility for all common space, private circulation systems, common parking lots, pedestrian paths, and other landscaped areas. Such a maintenance organization(s) may be public or private, or a combination of public and private.
- b. The maintenance organizations shall be established prior to Certificate of Occupancy. The ownership and maintenance responsibility shall be identified for each common area at the time Design Review or Conditional Use Permits are filed.

18. Structures shall be constructed to comply with California Energy Commission Title 24, Energy Efficiency Standards for Non-Residential Construction in place at the time the application for a building permit is submitted and complete.

19. In accordance with the California Solid Waste Reuse and Recycling Access Act of 1991, Cal Pub Resources Code §42911, adequate areas for collecting and loading recyclable materials shall be provided.

20. All businesses within Sunset Crossroads must provide recycling and organics recycling containers at front-of-house to collect waste generated from the products purchased and consumed on the premises consistent with CalRecycle requirements.

1. Any business generating 2 or more cubic yards of waste per week (recycling + organics recycling + disposal) shall engage in one of the following organic recycling activities:
 - a) Source separate organic waste from other waste and participate in a waste recycling service that includes collection and recycling of organic waste.
 - b) Recycle its organic waste on site, or self-haul its organic waste off site for recycling.
 - c) Subscribe to an organic waste recycling service that may include mixed waste processing that specifically recycles organic waste.
2. Any business that generates four cubic yards or more of commercial solid waste per week shall arrange for recycling services. Businesses can take one or any combination of the following actions in order to reuse, recycle, compost or otherwise divert solid waste from disposal:
 - a) Self-haul.
 - b) Subscribe to a hauler(s).
 - c) Arrange for the pickup of recyclable materials.
 - d) Subscribe to a recycling service that may include mixed waste processing that yields diversion results comparable to source separation.

3.2. PERMITTED, CONDITIONAL, ANCILLARY, AND PROHIBITED USES

Land within the SUNSET CROSSROADS Specific Plan and structures/facilities therein may be developed, and/or used for those activities listed in Table 3-1, *Permitted Uses*. Table 3-1 lists the Permitted, Conditionally Permitted, Ancillary and Prohibited land uses for each Planning Area established by this Specific Plan (Industrial, General Commercial, and Open Space). The Community Development Director is authorized to make a determination that a use not listed in Table 3-1 (*Permitted Uses*) may be allowed by the Specific Plan because it is found to be substantially similar to, and no more objectionable than, a Permitted, Conditionally Permitted, or Ancillary use, pursuant to the procedures in Chapter 5, *Implementation Plan*. Such a use shall be subject to the same review requirements as the identified "similar" allowed use. "Substantially similar" means that the use would be consistent with the purpose and intent of the Planning Area's Land Use Designation in which the use is proposed (refer to Chapter 2, *Development Plan*). The description of uses within Planning Areas 11 through 19 are provided in the footnotes of Table 3-1.

The City, in considering an application for a specific permitted use, shall determine whether there are significant environmental impacts resulting from such proposed use not addressed in the EIR or any then prepared subsequent environmental documentation. If the environmental impacts exceed the impacts disclosed in the EIR or subsequent environmental documentation approved by the City, the City shall undertake additional environmental review in compliance with CEQA prior to approving any such use.

The symbols shown in Table 3-1 have the following meanings:

- ❖ "P" means the land use is Permitted by right in the specified Land Use Designation, subject to the Implementation Regulations applicable to that Land Use Designation.
- ❖ "C" means the land use is Conditionally Permitted, subject to the filing of a Conditional Use Permit Application in accordance with the requirements of the City Zoning Ordinance and must be approved by the City of Banning before the use can be established within a particular Land Use Designation. A Conditional Use Permit application is an intensive consideration of an application submitted within this Specific Plan; no Design Review application is required.
- ❖ "A" means the land use is Permitted by right in the specified Land Use Designation only when the use is Ancillary (in support of) the main Permitted or Conditionally Permitted use of the property.
- ❖ "X" means the land use is Prohibited within that Land Use Designation.

Table 3-1 Permitted, Conditionally Permitted, Ancillary, and Prohibited Uses

| USE LEGEND: P: PERMITTED BY RIGHT C: CONDITIONAL USE A: ANCILLARY X: PROHIBITED | | | |
|---|---|--|-------|
| Use | General Commercial (Planning Area 1) | Industrial (Planning Areas 2, 3, 4, 5, 6, 7, 89 & 10) | Notes |
| Recreation, Education, and Public Assembly | | | |
| Go-Cart racetrack | P | X | |
| Billiard parlors/pool halls | C | X | |
| Religious Facilities | C | C | |
| Community centers | C | X | |
| Day care centers | P | A | |
| Health/fitness facilities | P | A | |
| Indoor recreation centers | P | X | |
| Libraries | P | P | |
| Membership organization facilities | P | C | |
| Museums | P | C | |
| Outdoor commercial recreation | P | X | |
| Public parks, outdoor spaces, and playgrounds | P | X | |
| Recreational vehicle (RV) storage | X | P | |
| Sport facilities and outdoor public assembly | C | X | |
| Studios for dance, art, music, photography, etc. | P | C | |
| Theatres and meeting halls | C | C | |
| Retail Uses | | | |
| Accessory retail uses | P | P | |
| Alcoholic beverage sales, on- or off-site | C | X | |
| Amusement Halls/Pools Halls | C | X | |
| Antique stores | P | X | |
| Art galleries | P | X | |
| Auto, mobile home, and motor vehicle sales, with or without part sales, new and/or used | X | C | |
| Auto, mobile home, and motor vehicle part sales | X | X | |
| Bakeries, retail | P | X | |
| Bars and drinking establishments | C | X | |
| Certified farmers' markets | P | P | |
| Convenience stores, with liquor sales | C | X | |
| Convenience stores, no liquor sales | P | X | |

| USE LEGEND: P: PERMITTED BY RIGHT C: CONDITIONAL USE A: ANCILLARY X: PROHIBITED | | | |
|---|---|--|---|
| Use | General Commercial (Planning Area 1) | Industrial (Planning Areas 2, 3, 4, 5, 6, 7, 89 & 10) | Notes |
| Department stores | P | X | |
| Drive-in/drive-through sales | C | X | |
| Drug stores | P | X | |
| Factory outlet centers | P | X | |
| Farm and ranch supply stores | P | X | |
| Furniture, furnishings, home equipment stores | P | X | |
| Gift shops | P | X | |
| Grocery stores, retail butchers and green grocers | P | X | |
| Hardware/lumber/building material stores | P | X | e.g., Ace Hardware, The Home Depot, etc. with corresponding ancillary outdoor storage for these uses. |
| Liquor stores (off-site consumption) | C | X | |
| Outdoor retail merchandise display and activities | C | X | |
| Pawn shops | X | X | |
| Pet stores and grooming | P | X | |
| Restaurants, no beer, wine or liquor sales | P | A | Ancillary to an industrial building i.e.: Cafeteria |
| Restaurants, serving beer, wine or liquor | P | A | Ancillary to an industrial building i.e.: Cafeteria |
| Restaurants, drive-in, take-out, fast food | P | A | Ancillary to an industrial building i.e.: Cafeteria |
| Retail stores, general merchandise | P | X | |
| Second hand/thrift stores | C | X | |
| Shopping centers, 15,000 square feet or more | P | X | |
| Warehouse or club stores (i.e., "Big box stores") | P | X | |
| Services | | | |
| Automatic teller machine (ATM), not at a bank | P | X | |
| Banks and financial establishments/services | P | X | |
| Barber shops and beauty shops | P | A | |
| Bed and breakfast establishments | C | X | |
| Business support/secretarial services | P | X | |

| USE LEGEND: P: PERMITTED BY RIGHT C: CONDITIONAL USE A: ANCILLARY X: PROHIBITED | | | |
|---|---|--|---|
| Use | General Commercial (Planning Area 1) | Industrial (Planning Areas 2, 3, 4, 5, 6, 7, 89 & 10) | Notes |
| Car wash | P | A | |
| Construction storage (indoor and/or outdoor) | X | C | |
| Drive-in and drive-through services | P | X | |
| Dry cleaning, retail | P | A | |
| Equipment rental yards | X | P | |
| Fortune-Telling | C | X | |
| Government offices | P | X | |
| Hookah and Smoking Lounges | C | C | |
| Hotels and motels, including extended stay | P | X | |
| Laundry, coin operated | P | X | |
| Massage Establishment | P | X | |
| Medical services, clinics and labs | C | X | |
| Professional offices | P | P | |
| Public parking | P | P | |
| Public utility and safety facilities | P | P | Including Fire Station, Reverse Osmosis Facility, Potable Water Reservoir, and Electrical Substation. |
| Real estate offices | P | X | |
| Repair and maintenance of consumer products | P | X | |
| Repair and maintenance of motor vehicles, including auto body | X | P | |
| Research and development facilities | X | P | |
| Storage, accessory, including self-storage | X | P | |
| Tattoo and Body Piercing Studios | C | X | |
| Travel Centers | P | X | Gas, Diesel, Electric, CNG, Car Wash, Retail/Fast Food |
| Urgent Care | P | X | |
| Vehicle Repair Facilities with or without eight (8) or fewer fueling stations [Automobile or Truck] | X | A | Limited to basic maintenance such as tire repairs, oil changes, and other minor non-engine vehicle maintenance. |
| Veterinary clinics, animal hospitals, grooming | P | X | |
| Manufacturing and Processing Uses | | | |
| Assembly from prefabricated parts or products | C | P | |
| Auto wrecking/parts salvaging | X | X | |

| USE LEGEND: P: PERMITTED BY RIGHT C: CONDITIONAL USE A: ANCILLARY X: PROHIBITED | | | |
|---|---|--|---|
| Use | General Commercial (Planning Area 1) | Industrial (Planning Areas 2, 3, 4, 5, 6, 7, 89 & 10) | Notes |
| Bakeries, wholesale | X | P | Wholesale only, no small bakeries. |
| Breweries and distilleries | P | P | |
| Breweries and distilleries, with bar and/or restaurant in same building. | P | P | |
| Cabinet shops, carpentry, furniture manufacturing and assembly | X | P | |
| Ceramics manufacturing and assembly | X | P | |
| Cleaning and dyeing plants | X | C | |
| Contractor's storage yards | X | P | |
| Creameries and dairies | X | C | |
| Distribution | X | P | |
| Food products manufacturing | X | P | |
| Furniture and fixtures | X | P | |
| Garment and shoe manufacturing | X | P | |
| Ice and cold storage | X | P | Not including cooled/chilled or frozen/foods/products distribution facilities |
| Laundries and dry-cleaning plants | C | P | |
| Lumber yards | X | P | Primarily wholesale to the trade, limited retail sales, outdoor storage. |
| Machining, welding and blacksmithing | X | P | |
| Manufacturing facilities, light | X | P | |
| Manufacturing facilities, heavy | X | X | |
| Metal plating shops | X | X | |
| Mixed-use office/industrial | X | P | |
| Mixed-use industrial/commercial | X | P | |
| Packing plant | X | X | |
| Printing/publishing | P | P | |
| Recycling facilities | X | P | |
| Recycling—Reverse vending machines | P | P | |
| Sheet metal shops | X | P | |
| Stone and granite storage and sales | X | P | |
| Storage yard | X | P | |
| Tire recapping, retreading and storage | X | C | |

| USE LEGEND: P: PERMITTED BY RIGHT C: CONDITIONAL USE A: ANCILLARY X: PROHIBITED | | | |
|--|---|--|--|
| Use | General Commercial (Planning Area 1) | Industrial (Planning Areas 2, 3, 4, 5, 6, 7, 89 & 10) | Notes |
| Truck (commercial) repair, towing, parking, storage and service | X | P | |
| Trucking yard or terminal | X | P | |
| Warehousing | X | P | |
| Wholesaling | X | P | |
| Other Uses | | | |
| Commercial or telecommunications antennae | P | P | |
| Public utility facilities | P | P | Including Electrical Substation, Fire Station, Potable Water Reservoir, and Reverse Osmosis Water Treatment Facility |
| Cannabis Commercial Indoor Cultivation ⁴ | X | C | |
| Cannabis Manufacturing Level 1 ⁴ | X | C | |
| Cannabis Retailer ⁴ | C | X | |
| Cannabis Testing Laboratory ⁴ | X | C | |
| Cannabis Distribution Facility ⁴ | X | C | |
| Plant nurseries, with on-site sales | P | X | |
| Plant nurseries, without on-site sales | C | X | |
| Commercial cargo/storage containers | C | P | |
| Additional Uses Not Listed in City's Ordinance | | | |
| Recreational Uses within fully enclosed building with ancillary outdoor activities. | P | P | |
| Linen and Uniform Supply | X | P | |
| Battery Energy Storage System (BESS) | X | P | |
| Distribution warehouses, general warehouses, and e-commerce fulfillment centers, for dry goods | X | P | |
| Distribution warehouses, general warehouses, and e-commerce fulfillment centers for chilled, cooled, or frozen goods | X | P | |
| Industrial Robotics Manufacturing and Assembly | X | P | |
| Data processing centers and data storage services | X | P | |
| Computer and electronic parts manufacturing and assembly | X | P | |

| USE LEGEND: P: PERMITTED BY RIGHT C: CONDITIONAL USE A: ANCILLARY X: PROHIBITED | | | |
|---|---|--|-------|
| Use | General Commercial (Planning Area 1) | Industrial (Planning Areas 2, 3, 4, 5, 6, 7, 89 & 10) | Notes |
| Machinery manufacture | X | P | |
| Day/health spa | P | X | |
| Freeway Oriented Freestanding Signs ³ | P | P | |
| Indoor Go Kart Racing Tracks | P | X | |
| Indoor Trampoline Parks and/or Indoor Obstacle Courses, Climbing Walls/Gyms | P | X | |
| Indoor, Man-made Surf and Wave Recreation | P | X | |

Notes:

¹ Uses permitted within Planning Area 11 include a 5.0-acre public park, natural open space, trails, and landscaping. Planning Area 12 shall include open space areas which may include trails, as well as a possible Reverse Osmosis Facility Site and Fire Station site.

² Development within Planning Areas 13, 14, 15, 16, 17, 18, and 19 shall be limited to grading associated with drainage crossings by roadways and/or fuel modification activities.

³ Freeway Oriented Freestanding Signs are permitted within Planning Areas 1, 6, and 7 and may be constructed as electronic/digital message board pylon signs consistent with the Specific Plan Sign Program.

⁴ Subject to Banning Municipal Code Requirements: Chapters 5.33, 5.35, 17.53 and 17.54.

3.3. GENERAL COMMERCIAL DEVELOPMENT STANDARDS

The standards identified below establish the development criteria that shall apply to Planning Area 1, designated General Commercial within SUNSET CROSSROADS. The Maximum Building Square Footage for Planning Area 1 is a total of 268,400 square feet of General Commercial building space in addition to a 125-room hotel (approximately 90,000 square feet), all at a Maximum Building Coverage of 50%. The 90,000 square feet of hotel use is not counted as part of the General Commercial's 268,400 Maximum Building Square Footage because traffic analysis for hotels is calculated by number of rooms, not by square footage. Design Guidelines for this Planning Area can be found in Chapter 4.

Table 3-2 Development Standards – General Commercial

| Site Requirements | |
|---|--|
| Minimum Lot Area | N/A |
| Minimum Average Lot Width | N/A |
| Maximum Building Coverage ¹ | 50% |
| Notes: | |
| 1. During implementation, the Maximum Building Coverage shall be calculated for each Planning Area or Parcel. The Specific Plan Maximum Building Square Footage for General Commercial uses of 268,400 s.f., excluding the hotel and fuel pumps, shall not be exceeded, regardless of the Maximum Building Coverage achieved in any Planning Area. The approximately 90,000 square feet of hotel use is not part of the General Commercial 268,400 Maximum Building Square Footage because traffic analysis for hotels is calculated by number of rooms, not by square footage. | |
| Minimum Setback Requirements (When abutting a street) (As measured from the building to the edge of the right-of-way or to the property line.) | |
| Front, Rear, and Side | Building Heights under 35 feet: None Building Heights over 35 feet: 2 feet for each foot the building height exceeds 35 feet |
| Architectural features such as cornices, eaves, canopies, decorative wall elements | May encroach up to 4 feet into the setback, provided that adequate emergency access can be maintained, subject to approval by the Riverside County Fire Department. |
| Minimum Setback Requirements at Interior Side Yards (as measured from the property line.) | |
| Building | 5 feet |
| Drive Aisle and Passenger Car and Truck Parking | 5 feet |
| Screened Loading and Storage Yards | 0 feet |
| Minimum Drive Aisle and Parking Space Separation Requirements | |
| Adjacent to building office element | 10 feet |
| Adjacent to solid building wall or screen wall/fence, and <u>not</u> within a screened/enclosed yard. | 5 feet |
| Adjacent to solid building wall or screen wall/fence, and within a screened/enclosed yard. | 0 feet |
| Other Development Standards | |
| Maximum Building Height (measured from the building's finished floor elevation to the highest point of the building's roof, parapet, solar arrays, or other architectural projection.) | 60 feet plus an additional 10 feet for solar arrays |
| Maximum Freeway Oriented Freestanding Sign | 80 feet |
| Ground and Roof-Mounted Equipment | All ground and roof mounted mechanical equipment shall be screened or obscured to the extent practical by landscaping when visible from abutting public roadways. |
| Signage | Signage shall be consistent with the Sign Program and Signage Design Guidelines, Signage Design Guidelines in the Specific Plan, or as approved by the City of Banning. |
| Outside Storage | If a non-screened outdoor general retail area is proposed, this area shall be identified on the implementing Design Review or Conditional Use Permit and shall be set back a minimum of 10 feet from the edge of the right-of-way. |

3.4. INDUSTRIAL DEVELOPMENT STANDARDS

The standards identified below establish the development criteria that shall apply to the Planning Areas designated Industrial within the SUNSET CROSSROADS Specific Plan (Planning Areas 2, 3, 4, 5, 6, 7, 8, 9, and 10). The combined Maximum Building Square Footage for Planning Areas 2, 3, 4, 5, 6, 7, 8, 9, and 10 is 5,545,000 square feet of Industrial building space, at a Maximum Building Coverage of 60%. Design Guidelines for these Planning Areas can be found in Chapter 4.

Table 3-3 Development Standards - Industrial

| Site Requirements | |
|---|--|
| Minimum Lot Area | N/A |
| Minimum Average Lot Width | N/A |
| Maximum Building Coverage ¹ | 60% |
| Notes: | |
| 1. During implementation, the Maximum Building Coverage shall be calculated for each Planning Area or Parcel. The Specific Plan maximum building square footage of 5,545,000 square feet for Industrial uses of shall not be exceeded, regardless of the Maximum Building Coverage achieved in any Planning Area. | |
| Minimum Setback Requirements (When abutting a street) (As measured from the building to the edge of right-of-way or property line.) | |
| Front | When abutting any public street – 25 feet (of which 10 feet adjacent to the right-of-way shall be landscaped) All other instances – 0 feet |
| Rear | 0 feet |
| Side | 0 feet |
| Architectural features such as cornices, eaves, canopies, decorative wall elements | May encroach up to 4 feet into the setback, provided that adequate emergency access can be maintained, subject to approval by the Riverside County Fire Department. |
| Minimum Setback Requirements at Interior Side Yards (as measured from the property line.) | |
| Building | 5 feet |
| Drive Aisle and Passenger Car and Truck Parking | 5 feet |
| Screened Loading and Storage Yards | 0 feet |
| Minimum Drive Aisle and Parking Space Separation Requirements | |
| Adjacent to building office element | 10 feet |
| Adjacent to solid building wall or screen wall/fence, and <u>not</u> within a screened/enclosed yard. | 5 feet |
| Adjacent to solid building wall or screen wall/fence, and within a screened/enclosed yard. | 0 feet |
| Other Development Standards | |
| Maximum Building Height (Planning Areas 3, 4, 6, 7, 8, & 9) (Measured from the building's finished floor elevation to the highest point of the building's roof, parapet, solar arrays, or other architectural projection.) | 60 feet plus an additional 10 feet for solar arrays |
| Maximum Building Height (Planning Areas 2 & 5) (Measured from the building's finished floor elevation to the highest point of the building's roof, parapet, solar arrays, or other architectural projection.) | Planning Area 2 - 60 feet plus an additional 10 feet for solar arrays, or architectural parapet or other solid screening, only when screening industrial refrigeration equipment Planning Area 5 - 80 feet |
| Maximum Freeway Oriented Freestanding Sign | 80 feet |
| Ground and Roof-Mounted Equipment | All ground and roof mounted mechanical equipment shall be screened or obscured by landscaping when visible from adjacent public roadways. |
| Signage | Signage shall be consistent with the Sign Program and Signage Design Guidelines, Signage Design Guidelines in the Specific Plan, or as approved by the City of Banning. |
| Outside Storage | If a non-screened outdoor storage area is proposed, the storage area shall be identified on the implementing Design Review and/or Conditional Use Permit and shall be set back a minimum of 10 feet from the edge of the right-of-way. |
| Loading Areas, Trailer Storage, Overhead Doors | Loading areas, trailer storage, and overhead doors may face toward a public street if either: (a) Located further than 100' from the street, measured to the edge of sidewalk or curb, or (b) Appropriately screened from view with any combination of berms and landscaping or decorative screen walls. |

3.4.1 BICYCLE PATHWAYS

1. Each Site Plan shall identify and the Project shall provide safe pathways for bicyclists from public roadways to on-site bicycle storage locations, using signage and/or striping.

3.4.2 TRUCK ROUTE MANAGEMENT PLAN REQUIREMENTS

1. Prior to the issuance of Final Occupancy Permits, each industrial site shall prepare and have approved by the Community Development Director, a Truck Management Plan containing the follow components:
 - a. Posting of signage clearly showing the designated entry for trucks from the public streets to the designated onsite truck check-in and truck parking areas.
 - b. Posting of signage indicating that all parking and maintenance of trucks must be conducted within the designated onsite areas and not within the surrounding community or on public streets.
 - c. Posting of signage for exiting traffic (other than exempt vehicles¹) showing the designated exits and restricting westward travel on Sun Lakes Boulevard west of Highland Home Road.
 - d. Lease provisions clearly identifying the required truck routes, including requiring trucks to use Sunset Avenue to access the I-10 Freeway interchange and prohibiting trucks (other than exempt vehicles¹) on Sun Lakes Boulevard west of Highland Home Road.
 - e. Consider and include, where feasible, driveway aprons providing egress to SLB Extension that physically direct trucks east on SLB Extension in a manner that does not affect exempt vehicles¹.
 - f. Truck route maps provided to all drivers and posted in breakrooms and throughout the Project.
 - g. Designation of a Traffic Coordinator contact for the City to notify in the event of traffic issues.

¹Exempt vehicles to include emergency and public safety vehicles, buses, limos and passenger vehicles, vehicles owned by a public utility or public agency and delivery vans serving local routes or using designated detour routes, tow trucks and moving vans.

3.5. PLANNING AREA STANDARDS

3.5.1 PLANNING AREA 1 [GENERAL COMMERCIAL (GC) – 47.9 ACRES]

a. DESCRIPTION

Planning Area 1 is designated for 47.9 acres of General Commercial land uses in the northeastern portion of SUNSET CROSSROADS and is designed as a regional draw, providing a leisure and entertainment destination focusing on health and wellness-based retail uses along with a 125-room hotel. A Travel Center is provided within Planning Area 1 to provide retail/convenience sales, drive-through fast food, vehicle fueling stations and commercial fueling stations to accommodate passenger vehicles, recreational vehicles, and multi-axle tractor trailers directly off the I-10.

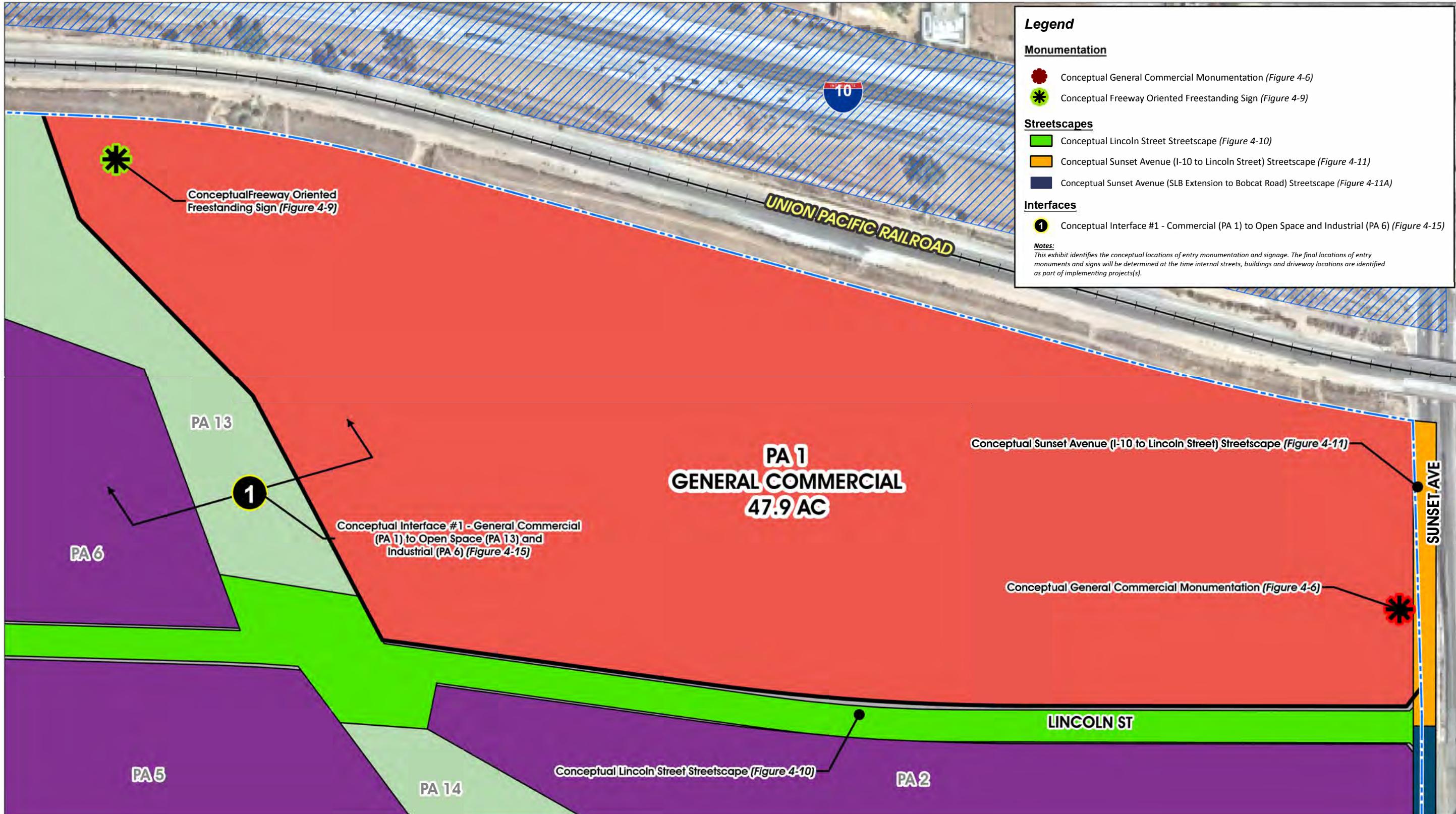
As shown on Figure 3-1, *Planning Area 1*, the General Commercial uses within Planning Area 1 are located north of Lincoln Street and west of Sunset Avenue. When entering the site from Sunset Avenue, visitors and employees are greeted with a Travel Center, restaurants and a multi-story hotel that initiates their experience at SUNSET CROSSROADS. Upon arrival, visitors and employees can park and continue to the heart of the development via pedestrian friendly walkways or continue into the Main Street District. Within the Main Street District, visitors will find wellness, restaurants, and recreational enterprises and outdoor gathering areas along both sides of A Street with patios, outdoor spaces, sports-oriented uses and retail storefronts.

The Main Street District leads directly to the heart of SUNSET CROSSROADS, terminating in a communal park surrounded by wellness-based retail, recreation and restaurants. Potential activity and wellness-based retail uses may

include stationary wave surfing, indoor skydiving, indoor go-karting, obstacle gyms, and an indoor/outdoor climbing wall that serves as a focal exterior icon to this district. A collection of smaller athletic and dance studio spaces and a green belt for yoga classes, family lawn games, and relaxing lounge seating areas contributes to the energy of this zone by allowing for indoor/outdoor connectivity of their programs into the outdoor gathering areas. It is this collection of unique wellness-based uses, restaurants, outdoor areas, communal and public open spaces, and public focal points that will define the vision of SUNSET CROSSROADS for years to come.

b. PLANNING AREA STANDARDS

1. Land uses within Planning Area 1 shall comply with the Permitted, Conditionally Permitted, Ancillary, and Prohibited Uses for General Commercial land uses provided in Section 3.2 and shall be consistent with Table 3-1.
2. Planning Area 1 shall comply with the Development Standards for General Commercial land uses provided in Section 3.3.
3. Primary access to Planning Area 1 shall be provided via Lincoln Street from Sunset Avenue, as shown on Figure 2-2, *Conceptual Circulation Plan*.
4. The locations of Primary Entry Monumentation, General Commercial Monumentation, Conceptual Tenant Monumentation, and Freeway Oriented Freestanding Signs for Planning Area 1 are conceptually illustrated on Figure 4-1, *Conceptual Master Landscape Plan*. The final location(s) of all monumentation and signage will be determined in conjunction with development of this Planning Area and each application submitted within this Specific Plan, and such locations shall be consistent with the Sign Program and Signage Design Guidelines. The Sign Program and Signage Design Guidelines shall provide guidance in determining the final locations of appropriate project, street, building, tenant identification, pedestrian path, and wayfinding signage for the anticipated variety of building sizes, designs, and uses.
5. Roadway landscape treatments shall be provided along Sunset Avenue and Lincoln Street, as conceptually illustrated on Figure 4-10, *Conceptual Lincoln Street Streetscape*, Figure 4-11, *Conceptual Sunset Avenue (I-10 to Lincoln Street) Streetscape*.
6. Walls and fencing along the exterior of Planning Area 1 may be provided. The location(s) and type of walls/fences interior to Planning Area 1 will be established in conjunction with an application submitted within this Specific Plan for this Planning Area.
7. The transition between Planning Area 1, Planning Area 13, and the Industrial uses located to the west within Planning Area 6, is conceptually illustrated in Figure 4-15, *Conceptual Interface #1 – Commercial (Planning Area 1) to Industrial (Planning Area 6)*.
8. Please refer to Section 3.1, *General Development Criteria*, for other applicable criteria that apply across the SUNSET CROSSROADS Specific Plan.
9. Please refer to Chapter 4, *Design Guidelines*, for other applicable design criteria.



Source(s): ESRI, Nearmap (2022), Proactive Engineering Consultants (01-2023), Hunter Landscape (2021), ADS (03-21-2021)

FIGURE 3-1

3.5.2 PLANNING AREA 2 [Industrial (i) – 33.8 acres]**a. DESCRIPTION**

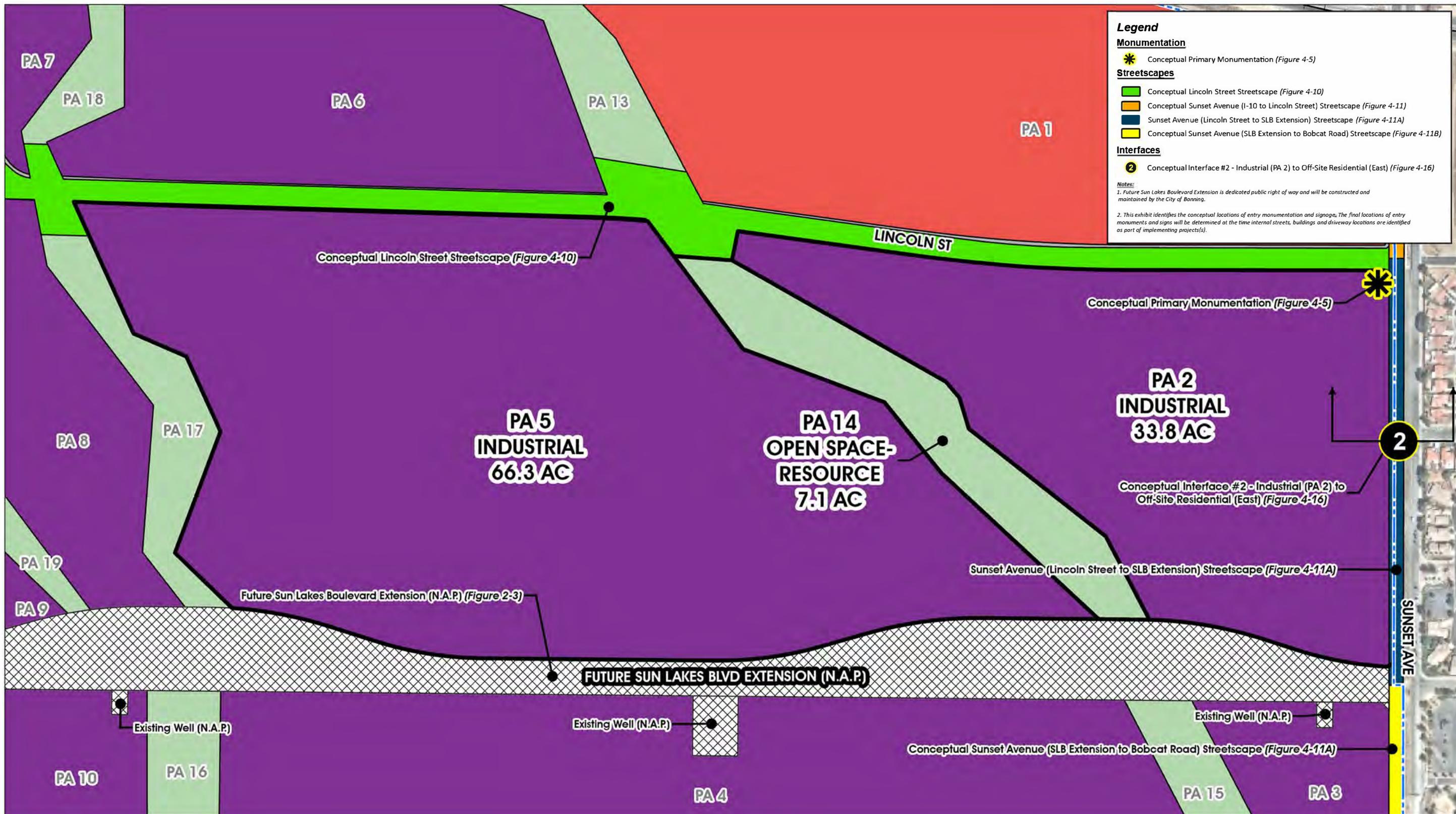
As shown on Figure 3-2, *Planning Areas 2, 5, and 14*, Planning Area 2 is located south of Lincoln Street, west of Sunset Avenue, and north of the SLB Extension and is designated for 33.8 acres of Industrial land uses. The full list of Permitted, Conditionally Permitted, Ancillary, and Prohibited Uses for this Planning Area is provided in Table 3-1.

Automobile and heavy commercial truck access to Planning Area 2 is provided via Lincoln Street and the SLB Extension. Connections to private driveways and drive aisles within Planning Area 2 will be designed and provided, as needed, in conjunction with development of this Planning Area.

Planning Area 2 may provide employees with building amenities in proximity to the Industrial uses. Such amenities may include, but are not limited to, shared outdoor patio break areas, pedestrian walkways, seating areas, and overhead structures for use by employees. Planning Area 2 provides decorative landscaping along its frontage with Lincoln Street and Sunset Avenue, as a visual amenity. Additional landscaping, setbacks, and orientation of truck bays will be considered during the design and implementation of this Planning Area to provide a buffer between the Industrial uses and existing residential to the east.

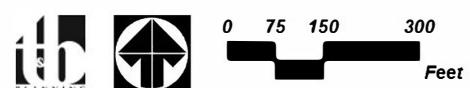
b. PLANNING AREA STANDARDS

1. Land uses within Planning Area 2 shall comply with the Permitted, Conditionally Permitted, Ancillary, and Prohibited Uses for Industrial land uses provided in Section 3.2 and shall be consistent with Table 3-1.
2. Planning Area 2 shall comply with the Development Standards for Industrial land uses provided in Section 3.4.
3. Industrial refrigeration equipment in Planning Area 2 may be ground-mounted or roof-mounted. When roof-mounted, such equipment shall be surrounded by a 10-foot-high architectural parapet or other solid screening.
4. Primary access to Planning Area 2 shall be provided via Lincoln Street and the SLB Extension, as shown on Figure 2-2, *Conceptual Circulation Plan*.
5. The locations of Primary Entry Monumentation and Industrial Monumentation for Planning Area 2 are conceptually illustrated on Figure 4-1, *Conceptual Master Landscape Plan*. The final location(s) of all monumentation will be determined in conjunction with development of this Planning Area and an application submitted within this Specific Plan, and such locations shall be consistent with the Sign Program and Signage Design Guidelines. The Sign Program and Signage Design Guidelines shall provide guidance in determining the final locations of appropriate project, street, building, tenant identification, pedestrian path, and wayfinding signage for the anticipated variety of building sizes, designs, and uses.
6. Roadway landscape treatments shall be provided along Sunset Avenue and Lincoln Street, as conceptually illustrated on Figure 4-10, *Conceptual Lincoln Street Streetscape*, Figure 4-11A, *Conceptual Sunset Avenue (Lincoln Street to SLB Extension) Streetscape*.
7. Walls and fencing along the exterior of Planning Area 2 may be provided. The location(s) and type of walls/fences interior to Planning Area 2 will be established in conjunction with development of this Planning Area.
8. A landscaped transition shall be provided between Planning Area 2 and the off-site residential uses located to the east, as conceptually illustrated in Figure 4-16, *Conceptual Interface #2 – Industrial (Planning Area 2) to Off-Site Residential (East)*.
9. Please refer to Section 3.1, *General Development Criteria*, for other applicable criteria that apply across the SUNSET CROSSROADS Specific Plan.
10. Please refer to Chapter 4, *Design Guidelines*, for other applicable design criteria.



Source(s): ESRI, Nearmap (2022), Proactive Engineering Consultants (01-2023), Hunter Landscape (2021), ADS (03-21-2021)

FIGURE 3-2



3.5.3 PLANNING AREA 3 [Industrial (i) – 7.8 acres]**a. DESCRIPTION**

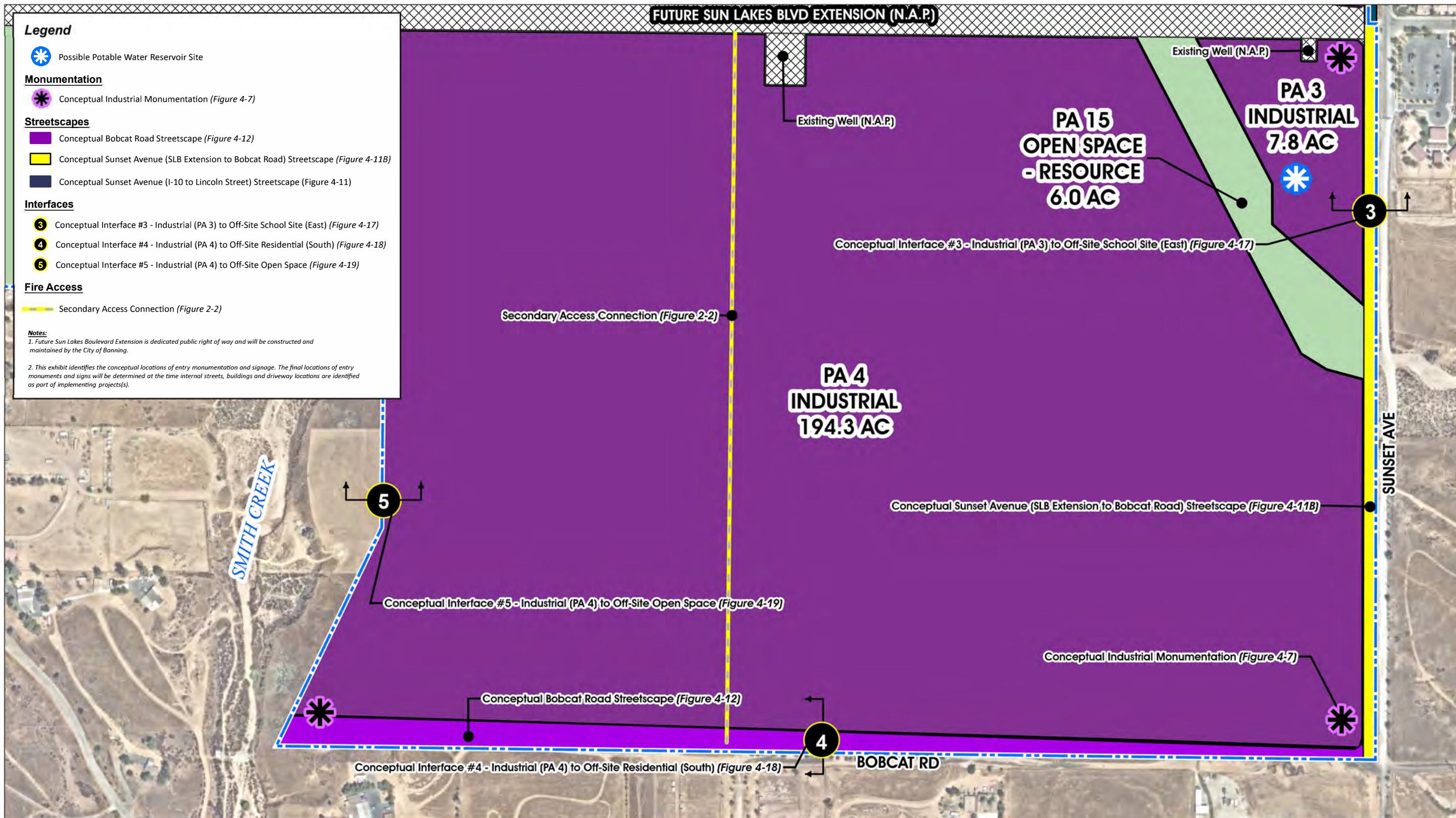
As shown on Figure 3-3, *Planning Areas 3, 4, and 15*, Planning Area 3 is located south of the SLB Extension and west of Sunset Avenue and is designated for 7.8 acres of Industrial land uses and may include a site for a Potable Water Reservoir. The full list of Permitted, Conditionally Permitted, Ancillary, and Prohibited Uses for this Planning Area is provided in Table 3-1, *Permitted Uses*.

Automobile and heavy commercial truck access to Planning Area 3 is provided via Sunset Avenue. Connections to private driveways and drive aisles within Planning Area 3 will be designed and provided, as needed, in conjunction with development of this Planning Area.

Planning Area 3 may provide employees with building amenities in proximity to the Industrial uses. Such amenities may include, but are not limited to, shared outdoor patio break areas, pedestrian walkways, seating areas, and overhead structures for use by employees. Planning Area 3 provides decorative landscaping along its frontage with Sunset Avenue as a visual amenity. Additional landscaping, setbacks, and orientation of truck bays will be considered during the design and implementation of this Planning Area to provide a buffer between the Industrial uses and existing/planned residential uses/MSJC Campus to the east.

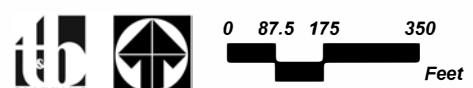
b. PLANNING AREA STANDARDS

1. Land uses within Planning Area 3 shall comply with the Permitted, Conditionally Permitted, Ancillary, and Prohibited Uses for Industrial land uses provided in Section 3.2 and shall be consistent with Table 3-1.
2. Planning Area 3 shall comply with the Development Standards for Industrial land uses provided in Section 3.4.
3. Primary access to Planning Area 3 shall be provided via Sunset Avenue, as shown on Figure 2-2, *Conceptual Circulation Plan*.
4. The locations of Primary Entry Monumentation and Industrial Monumentation for Planning Area 3 are conceptually illustrated on Figure 4-1, *Conceptual Master Landscape Plan*. The final location(s) of all monumentation will be determined in conjunction with development of this Planning Area and an application submitted within this Specific Plan, and such locations shall be consistent with the Sign Program and Signage Design Guidelines. The Sign Program and Signage Design Guidelines shall provide guidance in determining the final locations of appropriate project, street, building, tenant identification, pedestrian path, and wayfinding signage for the anticipated variety of building sizes, designs, and uses.
5. Roadway landscape treatments shall be provided along Sunset Avenue as conceptually illustrated on Figure 4-11B, *Conceptual Sunset Avenue Streetscape (SLB Extension to Bobcat Road)*.
6. Walls and fencing along the exterior of Planning Area 3 may be provided. The location(s) and type of walls/fences interior to Planning Area 3 will be established in conjunction with development of this Planning Area.
7. A landscaped transition shall be provided between Planning Area 3 and the off-site residential uses located to the east, as conceptually illustrated in Figure 4-17, *Conceptual Interface #3 – Industrial (Planning Area 3) to Off-Site School Site (East)*.
8. Please refer to Section 3.1, *General Development Criteria*, for other applicable criteria that apply across the SUNSET CROSSROADS Specific Plan.
9. Please refer to Chapter 4, *Design Guidelines*, for other applicable design criteria.



Source(s): ESRI, Nearmap (2022), Proactive Engineering Consultants (01-2023), Hunter Landscape (2023)

FIGURE 3-3



3.5.4 PLANNING AREA 4 [Industrial (I) – 194.3 acres]**a. DESCRIPTION**

As shown on Figure 3-3, *Planning Areas 3, 4, and 15*, Planning Area 4 is located north of Bobcat Road, south of the SLB Extension and west of Sunset Avenue and is designated for 194.3 acres of Industrial land uses. The full list of Permitted, Conditionally Permitted, Ancillary, and Prohibited Uses for this Planning Area is provided in Table 3-1.

Automobile and heavy commercial truck access to Planning Area 4 is provided via the SLB Extension and Sunset Avenue. Connections to private driveways and drive aisles within Planning Area 4 will be designed and provided, as needed, in conjunction with development of this Planning Area.

Planning Area 4 may provide employees with building amenities in proximity to the Industrial uses. Such amenities may include, but are not limited to, shared outdoor patio break areas, pedestrian walkways, seating areas, and overhead structures for use by employees. Planning Area 4 provides decorative landscaping along its frontage with Sunset Avenue and Bobcat Road as a visual amenity.

b. PLANNING AREA STANDARDS

1. Land uses within Planning Area 4 shall comply with the Permitted, Conditionally Permitted, Ancillary, and Prohibited Uses for Industrial land uses provided in Section 3.2 and shall be consistent with Table 3-1.
2. Planning Area 4 shall comply with the Development Standards for Industrial land uses provided in Section 3.4.
3. In Planning Area 4, 6-foot-high walls shall be constructed around the west, south, and east perimeters of parking lots fronting Bobcat Road across from residential uses. Buildings with parking lots that front Bobcat Road shall be constructed with 10-foot-high and 100-to-175-foot-long “wing walls” to reduce on-site operational noise.
4. Primary access to Planning Area 4 shall be provided via Sunset Avenue and the SLB Extension, as shown on Figure 2-2, *Conceptual Circulation Plan*.
5. The locations of Primary Entry Monumentation and Industrial Monumentation for Planning Area 4 are conceptually illustrated on Figure 4-1, *Conceptual Master Landscape Plan*. The final location(s) of all monumentation will be determined in conjunction with development of this Planning Area and an application submitted within this Specific Plan, and such locations shall be consistent with the Sign Program and Signage Design Guidelines. The Sign Program and Signage Design Guidelines shall provide guidance in determining the final locations of appropriate project, street, building, tenant identification, pedestrian path, and wayfinding signage for the anticipated variety of building sizes, designs, and uses.
6. Roadway landscape treatments shall be provided along Sunset Avenue as conceptually illustrated on Figure 4-11B, *Conceptual Sunset Avenue Streetscape (SLB Extension to Bobcat Road)*, and Figure 4-12, *Conceptual Bobcat Road Streetscape*.
7. Walls and fencing along the exterior of Planning Area 4 may be provided. The location(s) and type of walls/fences interior to Planning Area 4 will be established in conjunction with development of this Planning Area.
8. A landscaped transition shall be provided between Planning Area 4 and the off-site residential uses located to the south, as conceptually illustrated in Figure 4-18, *Conceptual Interface #4 – Industrial (Planning Area 4) to Off-Site Residential (South)*.
9. A landscaped transition shall be provided between Planning Area 4 and the off-site vacant land located to the west, as conceptually illustrated in Figure 4-19, *Conceptual Interface #5 – Industrial (Planning Area 4) to Off-Site Open Space*.
10. Please refer to Section 3.1, *General Development Criteria*, for other applicable criteria that apply across the SUNSET CROSSROADS Specific Plan.

11. Please refer to Chapter 4, *Design Guidelines*, for other applicable design criteria.

3.5.5 PLANNING AREA 5 [Industrial (I) – 66.3 acres]

a. DESCRIPTION

As shown on Figure 3-2, *Planning Areas 2, 5, and 14*, Planning Area 5 is located south of Lincoln Street and north of the SLB Extension, between Planning Areas 14 and 17 and is designated for 66.3 acres of Industrial land uses. The full list of Permitted, Conditionally Permitted, Ancillary and Prohibited Uses for this Planning Area is provided in Table 3-1.

Automobile and heavy commercial truck access to Planning Area 5 is provided via Lincoln Street and the SLB Extension. Connections to private driveways and drive aisles within Planning Area 5 will be designed and provided, as needed, in conjunction with development of this Planning Area.

Planning Area 5 may provide employees with building amenities in proximity to the Industrial uses. Such amenities may include, but are not limited to, shared outdoor patio break areas, pedestrian walkways, seating areas, and overhead structures for use by employees. Planning Area 5 provides decorative landscaping along its frontage with Lincoln Street as a visual amenity.

b. PLANNING AREA STANDARDS

1. Land uses within Planning Area 5 shall comply with the Permitted, Conditionally Permitted, Ancillary, and Prohibited Uses for Industrial land uses provided in Section 3.2 and shall be consistent with Table 3-1.
2. Planning Area 5 shall comply with the Development Standards for Industrial land uses provided in Section 3.4.
3. Primary access to Planning Area 5 shall be provided via Sunset Avenue and the SLB Extension, as shown on Figure 2-2, *Conceptual Circulation Plan*.
4. The locations of Primary Entry Monumentation and Industrial Monumentation for Planning Area 5 are conceptually illustrated on Figure 4-1, *Conceptual Master Landscape Plan*. The final location(s) of all monumentation will be determined in conjunction with development of this Planning Area and an application submitted within this Specific Plan, and such locations shall be consistent with the Sign Program and Signage Design Guidelines. The Sign Program and Signage Design Guidelines shall provide guidance in determining the final locations of appropriate project, street, building, tenant identification, pedestrian path, and wayfinding signage for the anticipated variety of building sizes, designs, and uses.
5. Walls and fencing along the exterior of Planning Area 5 may be provided. The location(s) and type of walls/fences interior to Planning Area 5 will be established in conjunction with development of this Planning Area.
6. Please refer to Section 3.1, *General Development Criteria*, for other applicable criteria that apply across the SUNSET CROSSROADS Specific Plan.
7. Please refer to Chapter 4, *Design Guidelines*, for other applicable design criteria.

3.5.6 PLANNING AREA 6 [INDUSTRIAL (I) – 28.1 ACRES]**a. DESCRIPTION**

As shown on Figure 3-4, *Planning Areas 6, 7, 13, and 18*, Planning Area 6 is located north of Lincoln Street, between Planning Areas 13 and 18 and is designated for 28.1 acres of Industrial land uses. The full list of Permitted, Conditionally Permitted, Ancillary, and Prohibited Uses for this Planning Area is provided in Table 3-1.

Automobile and heavy commercial truck access to Planning Area 6 is provided via Lincoln Street. Connections to private driveways and drive aisles within Planning Area 6 will be designed and provided, as needed, in conjunction with development of this Planning Area.

Planning Area 6 may provide employees with building amenities in proximity to the Industrial uses. Such amenities may include, but are not limited to, shared outdoor patio break areas, pedestrian walkways, seating areas, and overhead structures for use by employees. Planning Area 6 provides decorative landscaping along its frontage with Lincoln Street as a visual amenity.

b. PLANNING AREA STANDARDS

1. Land uses within Planning Area 6 shall comply with the Permitted, Conditionally Permitted, Ancillary, and Prohibited Uses for Industrial land uses provided in Section 3.2 and shall be consistent with Table 3-1.
2. Planning Area 6 shall comply with the Development Standards for Industrial land uses provided in Section 3.4.
3. Primary access to Planning Area 6 shall be provided via Lincoln Street, as shown on Figure 2-2, *Conceptual Circulation Plan*.
4. The locations of Primary Entry Monumentation and Industrial Monumentation for Planning Area 6 are conceptually illustrated on Figure 4-1, *Conceptual Master Landscape Plan*. The final location(s) of all monumentation will be determined in conjunction with development of this Planning Area and an application submitted within this Specific Plan, and such locations shall be consistent with the Sign Program and Signage Design Guidelines. The Sign Program and Signage Design Guidelines shall provide guidance in determining the final locations of appropriate project, street, building, tenant identification, pedestrian path, and wayfinding signage for the anticipated variety of building sizes, designs, and uses.
5. Roadway landscape treatments shall be provided along Lincoln Street as conceptually illustrated on Figure 4-10, *Conceptual Lincoln Street Streetscape*.
6. Walls and fencing along the exterior of Planning Area 6 may be provided. The location(s) and type of walls/fences interior to Planning Area 6 will be established in conjunction with development of this Planning Area.
7. Please refer to Section 3.1, *General Development Criteria*, for other applicable criteria that apply across the SUNSET CROSSROADS Specific Plan.
8. Please refer to Chapter 4, *Design Guidelines*, for other applicable design criteria.



Source(s): ESRI, Nearmap (2022), Proactive Engineering Consultants (09-2021), Hunter Landscape (2021)

FIGURE 3-4

3.5.7 PLANNING AREA 7 [INDUSTRIAL (I) – 25.8 ACRES]**a. DESCRIPTION**

As shown on Figure 3-4, *Planning Areas 6, 7, 13, and 18*, Planning Area 7 is located west of Planning Area 18 and east of Highland Home Road and is designated for 25.8 acres of Industrial land uses. The full list of Permitted, Conditionally Permitted, Ancillary, and Prohibited Uses for this Planning Area is provided in Table 3-1.

Automobile and heavy commercial truck access to Planning Area 7 is provided via Lincoln Street with additional access provided via the 24'-wide Secondary Access Connection from Highland Home Road. Connections to private driveways and drive aisles within Planning Area 7 will be designed and provided, as needed, in conjunction with development of this Planning Area.

Planning Area 7 may provide employees with building amenities in proximity to the Industrial uses. Such amenities may include, but are not limited to, shared outdoor patio break areas, pedestrian walkways, seating areas, and overhead structures for use by employees. Planning Area 7 may include a City-constructed 1.0-acre Electrical Substation Site in the northwest corner of the site, accessible via a paved 24' access road within Planning Area 7. Planning Area 7 provides an open space, landscaped buffer along its frontage with Highland Home Road as a visual amenity.

b. PLANNING AREA STANDARDS

1. Land uses within Planning Area 7 shall comply with the Permitted, Conditionally Permitted, Ancillary, and Prohibited Uses for Industrial land uses provided in Section 3.2 and shall be consistent with Table 3-1.
2. Planning Area 7 shall comply with the Development Standards for Industrial land uses provided in Section 3.4.
3. Primary access to Planning Area 7 shall be provided via Lincoln Street, with secondary access via a 24' Secondary Access Connection to Highland Home Road, as shown on Figure 2-2, *Conceptual Circulation Plan*.
4. The locations of Primary Entry Monumentation and Industrial Monumentation for Planning Area 7 are conceptually illustrated on Figure 4-1, *Conceptual Master Landscape Plan*. The final location(s) of all monumentation will be determined in conjunction with development of this Planning Area and an application submitted within this Specific Plan, and such locations shall be consistent with the Sign Program and Signage Design Guidelines. The Sign Program and Signage Design Guidelines shall provide guidance in determining the final locations of appropriate project, street, building, tenant identification, pedestrian path, and wayfinding signage for the anticipated variety of building sizes, designs, and uses.
5. Roadway landscape treatments shall be provided along Highland Home Road as conceptually illustrated on Figure 4-13, *Conceptual Highland Home Road Streetscape*.
6. Walls and fencing along the exterior of Planning Area 7 may be provided. The location(s) and type of walls/fences interior to Planning Area 7 will be established in conjunction with development of this Planning Area.
7. A landscaped transition, a minimum of 100' deep along the western boundary shall be provided between industrial parking areas in Planning Area 7 and the off-site uses located to the west, as conceptually illustrated in Figure 4-21, *Conceptual Interface #7 – Industrial (Planning Area 7) to Off-Site Residential (West)*. With the exception of a potential electrical substation, a minimum 100' buffer along the western boundary of Planning Area 7 will be provided and considered during the design and implementation of this Planning Area.
8. Please refer to Section 3.1, *General Development Criteria*, for other applicable criteria that apply across the SUNSET CROSSROADS Specific Plan.

Please refer to Chapter 4, *Design Guidelines*, for other applicable design criteria.

3.5.8 PLANNING AREA 8 [INDUSTRIAL (I) – 16.2 ACRES]**a. DESCRIPTION**

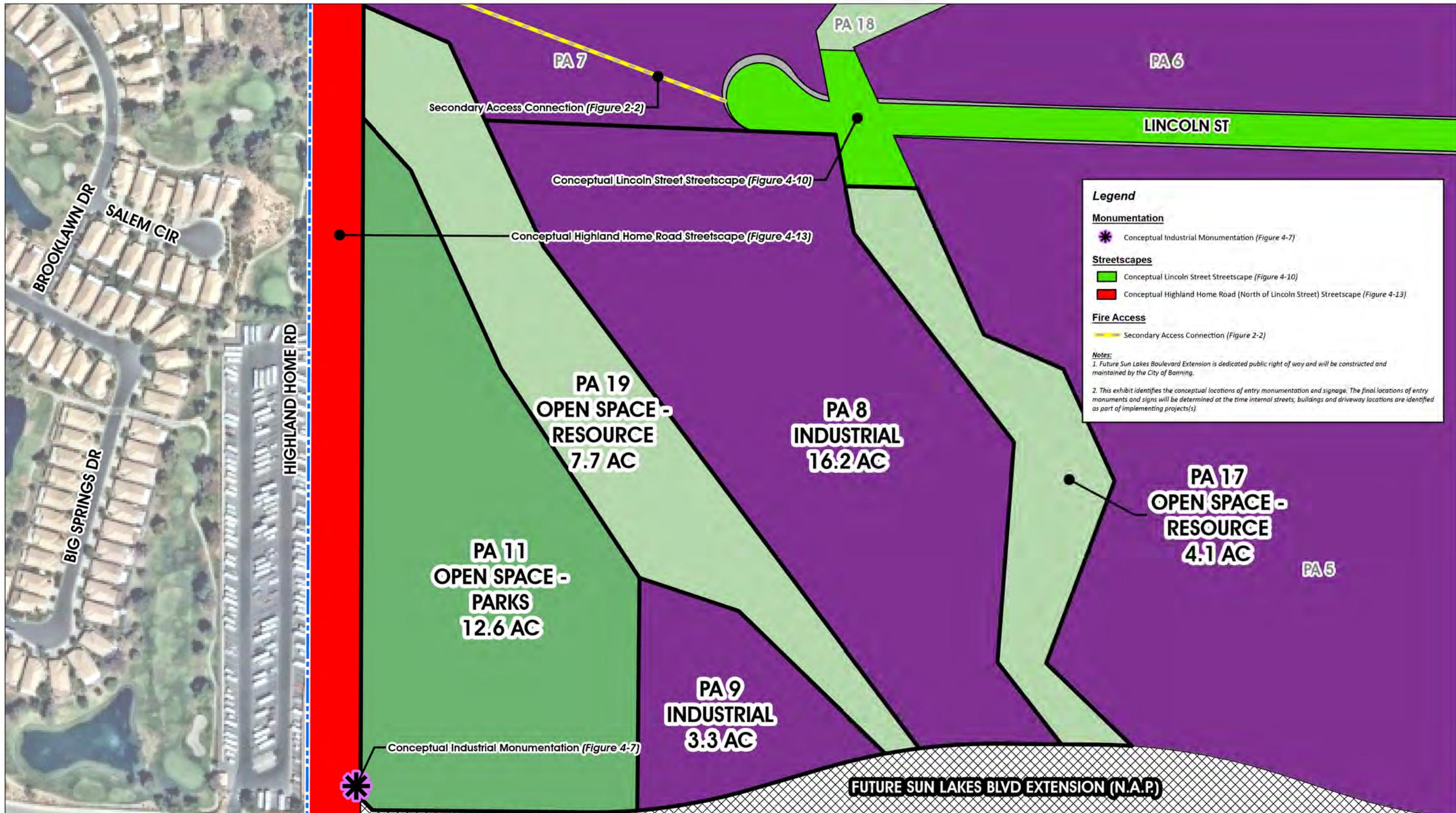
As shown on Figure 3-5, *Planning Areas 8, 9, 11, 17, and 19*, Planning Area 8 is located south of Lincoln Street and north of the SLB Extension, between Planning Areas 17 and 19 and is designated for 16.2 acres of Industrial land uses. The full list of Permitted, Conditionally Permitted, Ancillary, and Prohibited Uses for this Planning Area is provided in Table 3-1.

Automobile and heavy commercial truck access to Planning Area 8 is provided via Lincoln Street and the SLB Extension. Connections to private driveways and drive aisles within Planning Area 8 will be designed and provided, as needed, in conjunction with development of this Planning Area.

Planning Area 8 may provide employees with building amenities in proximity to the Industrial uses. Such amenities may include, but are not limited to, shared outdoor patio break areas, pedestrian walkways, seating areas, and overhead structures for use by employees. Planning Area 8 provides decorative landscaping along its frontage with Lincoln Street as a visual amenity.

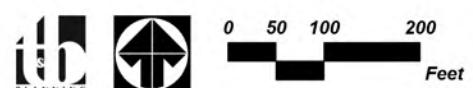
b. PLANNING AREA STANDARDS

1. Land uses within Planning Area 8 shall comply with the Permitted, Conditionally Permitted, Ancillary, and Prohibited Uses for Industrial land uses provided in Section 3.2 and shall be consistent with Table 3-1.
2. Planning Area 8 shall comply with the Development Standards for Industrial land uses provided in Section 3.4.
3. Primary access to Planning Area 8 shall be provided via Lincoln Street and the SLB Extension via Sunset Avenue, as shown on Figure 2-2, *Conceptual Circulation Plan*.
4. The locations of Primary Entry Monumentation and Industrial Monumentation for Planning Area 8 are conceptually illustrated on Figure 4-1, *Conceptual Master Landscape Plan*. The final location(s) of all monumentation will be determined in conjunction with development of this Planning Area and an application submitted within this Specific Plan, and such locations shall be consistent with the Sign Program and Signage Design Guidelines. The Sign Program and Signage Design Guidelines shall provide guidance in determining the final locations of appropriate project, street, building, tenant identification, pedestrian path, and wayfinding signage for the anticipated variety of building sizes, designs, and uses.
5. Roadway landscape treatments shall be provided along Lincoln Street as conceptually illustrated on Figure 4-10, *Conceptual Lincoln Street Streetscape*.
6. Walls and fencing along the exterior of Planning Area 8 may be provided. The location(s) and type of walls/fences interior to Planning Area 8 will be established in conjunction with development of this Planning Area.
7. Please refer to Section 3.1, *General Development Criteria*, for other applicable criteria that apply across the SUNSET CROSSROADS Specific Plan.
8. Please refer to Chapter 4, *Design Guidelines*, for other applicable design criteria.



Source(s): ESRI, Nearmap (2022), Proactive Engineering Consultants (01-2023), Hunter Landscape (2021), ADS (03-21-2021)

FIGURE 3-5



3.5.9 PLANNING AREA 9 [Industrial (i) – 3.3 acres]**a. DESCRIPTION**

As shown on Figure 3-5, *Planning Areas 8, 9, 11, 17, and 19*, Planning Area 9 is located north of the SLB Extension, between Planning Areas 11 and 19 and is designated for 3.3 acres of Industrial land uses. The full list of Permitted, Conditionally Permitted, Ancillary, and Prohibited Uses for this Planning Area is provided in Table 3-1.

Automobile and heavy commercial truck access to Planning Area 9 is provided via the SLB Extension. Connections to private driveways and drive aisles within Planning Area 9 will be designed and provided, as needed, in conjunction with development of this Planning Area.

Planning Area 9 may provide employees with building amenities in proximity to the Industrial uses. Such amenities may include, but are not limited to, shared outdoor patio break areas, pedestrian walkways, seating areas, and overhead structures for use by employees.

b. PLANNING AREA STANDARDS

1. Land uses within Planning Area 9 shall comply with the Permitted, Conditionally Permitted, Ancillary, and Prohibited Uses for Industrial land uses provided in Section 3.2 and shall be consistent with Table 3-1.
2. Planning Area 9 shall comply with the Development Standards for Industrial land uses provided in Section 3.4.
3. Primary access to Planning Area 9 shall be provided via the SLB Extension, as shown on Figure 2-2, *Conceptual Circulation Plan*.
4. The locations of Primary Entry Monumentation and Industrial Monumentation for Planning Area 9 are conceptually illustrated on Figure 4-1, *Conceptual Master Landscape Plan*. The final location(s) of all monumentation will be determined in conjunction with development of this Planning Area and an application submitted within this Specific Plan, and such locations shall be consistent with the Sign Program and Signage Design Guidelines. The Sign Program and Signage Design Guidelines shall provide guidance in determining the final locations of appropriate project, street, building, tenant identification, pedestrian path, and wayfinding signage for the anticipated variety of building sizes, designs, and uses.
5. Walls and fencing along the exterior of Planning Area 9 may be provided. The location(s) and type of walls/fences interior to Planning Area 9 will be established in conjunction with development of this Planning Area.
6. Please refer to Section 3.1, *General Development Criteria*, for other applicable criteria that apply across the SUNSET CROSSROADS Specific Plan.
7. Please refer to Chapter 4, *Design Guidelines*, for other applicable design criteria.

3.5.10 PLANNING AREA 10 [INDUSTRIAL (I) – 16.4 ACRES]**a. DESCRIPTION**

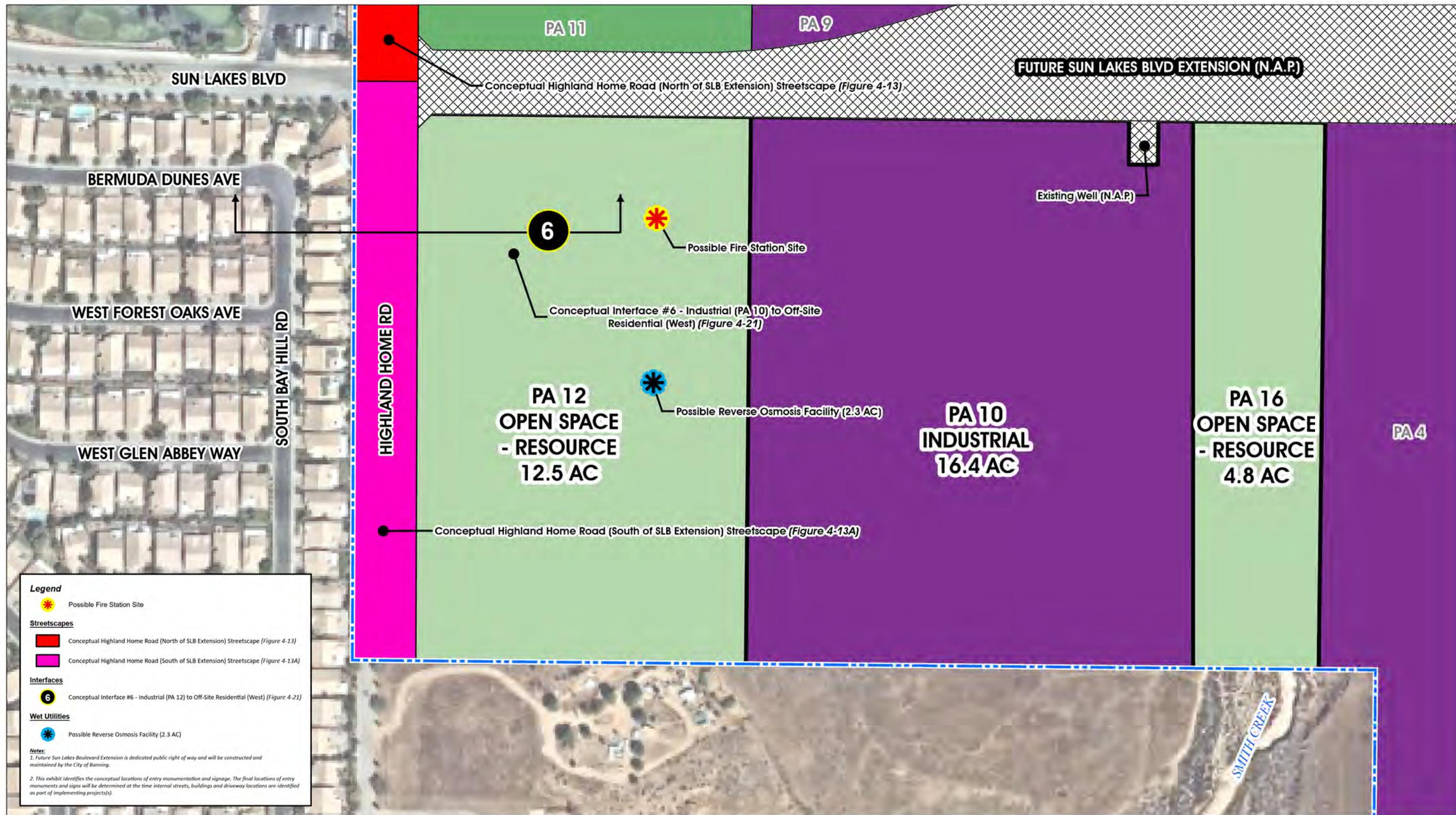
As shown on Figure 3-6, *Planning Areas 10, 12, and 16*, Planning Area 10 is located south of the SLB Extension, between Planning Areas 12 and 16 and is designated for 16.4 acres of Industrial land uses. The full list of Permitted, Conditionally Permitted, Ancillary, and Prohibited Uses for this Planning Area is provided in Table 3-1.

Automobile and heavy commercial truck access to Planning Area 10 is provided via the SLB Extension. Connections to private driveways and drive aisles within Planning Area 10 will be designed and provided, as needed, in conjunction with development of this Planning Area.

Planning Area 10 may provide employees with building amenities in proximity to the Industrial uses. Such amenities may include, but are not limited to, shared outdoor patio break areas, pedestrian walkways, seating areas, and overhead structures for use by employees.

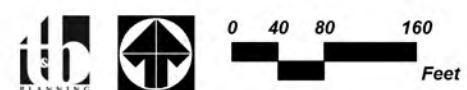
b. PLANNING AREA STANDARDS

1. Land uses within Planning Area 10 shall comply with the Permitted, Conditionally Permitted, Ancillary, and Prohibited Uses for Industrial land uses provided in Section 3.2 and shall be consistent with Table 3-1.
2. Planning Area 10 shall comply with the Development Standards for Industrial land uses provided in Section 3.4.
3. Primary access to Planning Area 10 shall be provided via the SLB Extension, as shown on Figure 2-2, *Conceptual Circulation Plan*.
4. The locations of Primary Entry Monumentation and Industrial Monumentation for Planning Area 10 are conceptually illustrated on Figure 4-1, *Conceptual Master Landscape Plan*. The final location(s) of all monumentation will be determined in conjunction with development of this Planning Area and an application submitted within this Specific Plan, and such locations shall be consistent with the Sign Program and Signage Design Guidelines. The Sign Program and Signage Design Guidelines shall provide guidance in determining the final locations of appropriate project, street, building, tenant identification, pedestrian path, and wayfinding signage for the anticipated variety of building sizes, designs, and uses.
5. Walls and fencing along the exterior of Planning Area 10 may be provided. The location(s) and type of walls/fences interior to Planning Area 10 will be established in conjunction with development of this Planning Area.
6. Please refer to Section 3.1, *General Development Criteria*, for other applicable criteria that apply across the SUNSET CROSSROADS Specific Plan.
7. Please refer to Chapter 4, *Design Guidelines*, for other applicable design criteria.



Source(s): ESRI, Nearmap (2022), Proactive Engineering Consultants (01-2023), Hunter Landscape (2021)

FIGURE 3-6



3.5.11 PLANNING AREA 11 [OPEN SPACE – PARKS (OS-P) – 12.6 ACRES]

a. DESCRIPTION

As shown on Figure 3-5, *Planning Areas 8, 9, 11, 17, and 19*, Planning Area 11 consists of 12.6 acres designated as Open Space-Parks in the western portion of SUNSET CROSSROADS. Planning Area 11 is intended to include a publicly-accessible 5.0-acre passive park, natural open space, fuel modification areas, trails, and landscaping. Planning Area 11 may provide recreational amenities such as, but not limited to, a tot lot playground, picnic tables, trails, walking paths, a parking lot, and restrooms. Open field areas are provided next to the playground and picnic tables, along with a looped walking path which winds through the recreational area. The 5.0-acre park portion of Planning Area 11 will be dedicated to and maintained by the City of Banning, and the remaining open space portion will be maintained by the Master Property Owners' Association or a Public or semi-public Agency.

As shown on Figure 3-5, access to Planning Area 11 is provided from the SLB Extension. Planning Area 11 is bordered by Planning Areas 9 and 19 to the east, the SLB Extension to the south, and Highland Home Road to the west. Planning Area 11 provides a buffer between the Industrial land uses of in Planning Areas 9, and the off-site residential land uses to the west.

b. PLANNING AREA STANDARDS

1. Primary access to Planning Area 11 shall be provided via the SLB Extension, as shown on Figure 2-2, *Conceptual Circulation Plan*.
2. Low-level security lighting may be provided for the park, tot lot playground, trails, parking lot, and restrooms. The trails and parking lot may include bollard lighting while the tot lot playground and restrooms may include security lighting. High intensity sports-field lighting is prohibited.
3. Walls and fencing within Planning Area 11 may be provided as a visual and physical buffer between the off-site residential uses, Industrial Uses, and/or existing natural drainage features. The location(s) of walls/fences interior to Planning Area 11 will be established in conjunction with development of this Planning Area.
4. Please refer to Section 3.1, *General Development Criteria*, for other applicable criteria that apply across the SUNSET CROSSROADS Specific Plan.
5. Please refer to Chapter 4, *Design Guidelines*, for other applicable design criteria.

3.5.12 PLANNING AREA 12 [OPEN SPACE – RESOURCE (OS-R) – 12.5 ACRES]

a. DESCRIPTION

As shown on Figure 3-6, *Planning Areas 10, 12, and 16*, Planning Area 12 consists of 12.5 acres designated as Open Space-Resource within SUNSET CROSSROADS. Planning Area 12 is intended to serve as a buffer for the Sun Lakes residential community to the west and includes open space areas which may contain trails as well as a City-constructed 2.3-acre site for a Reverse Osmosis Facility Site and a 1.5- acre site for a future Fire Station. The open space portions of Planning Area 12 will be dedicated to the City of Banning and maintained by a Master Property Owners' Association or Public Agency.

b. PLANNING AREA STANDARDS

1. Please refer to Section 3.1, *General Development Criteria*, for other applicable criteria that apply across the SUNSET CROSSROADS Specific Plan.
2. A landscaped transition shall be provided between Planning Area 12 and the off-site residential uses located to the south, as conceptually illustrated in Figure 4-20, Interface #6 – Industrial (Planning Area 12) to Off-Site Residential (West).
3. Please refer to Chapter 4, *Design Guidelines*, for other applicable design criteria

3.5.13 PLANNING AREAS 13, 14, 15, 16, 17, 18, & 19 [OPEN SPACE – RESOURCE (OS-R) – 40.5 ACRES]

b. DESCRIPTION

As shown on Figure 3-2, Figure 3-3, Figure 3-4, Figure 3-5, and Figure 3-6, Planning Areas 13, 14, 15, 16, 17, 18, and 19 consists of a combined 40.5 acres designated as Open Space-Resource within SUNSET CROSSROADS. Planning Areas 13, 14, 15, 16, 17, 18, and 19 are intended to preserve the existing natural drainage features of the Specific Plan site. The Property Owners' Association (POA) will maintain the drainage features consistent with the mitigation requirements, maintain the local interface with the natural drainages and storm drain discharge locations, and maintain access to storm drain outlet structures. Development within Planning Area 13, 14, 15, 16, 17, 18, and 19 shall be limited to grading, bridge crossings, and fuel modification areas.

b. PLANNING AREA STANDARDS

1. Walls and fencing within Planning Areas 13 through 19 may be provided as a visual and physical buffer between the existing natural drainage features and the General Commercial, Industrial, and Open Space-Park land uses. The location(s) of walls/fences abutting Planning Areas 13 through 19 will be established in conjunction with the development of adjacent Industrial and General Commercial Planning Areas, respectively.
2. Please refer to Section 3.1, *General Development Criteria*, for other applicable criteria that apply across the SUNSET CROSSROADS Specific Plan.
3. Please refer to Chapter 4, *Design Guidelines*, for other applicable design criteria.

3.6. ENERGY EFFICIENCY DEVELOPMENT CRITERIA

Development within SUNSET CROSSROADS is expected to be Energy Efficient consistent with or exceeding the criteria from the California Building Code, Title 24 standards which the City has adopted by ordinance. The Developer and City of Banning recognize that the technological and methodological specifications in the criteria could become obsolete in the future due to advancement over time. In that event, SUNSET CROSSROADS may implement new technologies and methodologies if they achieve at least as much environmental protection and do not result in new or greater significant environmental impacts than the technologies or methodologies specified in the following criteria:

1. Energy Efficient Structures

- a. Enhanced Insulation shall be provided via methods such as rigid wall insulation R-15, roof/attic R-30, etc.
- b. Greatly Enhanced Window Insulation with 0.28 or less U-factor, 0.22 or less SHGC, etc. shall be provided.
- c. Modest Cool Roofs with CRRC Rated 0.15 aged solar reflectance, 0.75 thermal emittance, etc. shall be provided.
- d. Building roofs shall be in compliance with solar requirements of the California Building Code, Title 24 standards.

2. Energy Efficient Heating and Cooling (HVAC)

- a. Improved Efficiency HVAC (EER 14/78% AFUE or 8 HSPF) shall be provided.
- b. Office space heating within warehouses must utilize heat pumps.

3. Energy Efficient Potable Water

- a. Improved Efficiency Water Heater (0.72 Energy Factor) shall be provided.
- b. Water Efficient Showerheads (1.8 gpm) shall be provided.
- c. Water Efficient Toilets/Urinals (1.28 gpm) shall be provided.
- d. Water Efficient Faucets (0.5 gpm for lavatories; 1.8 gpm for kitchens) shall be provided.
- e. Water Efficient Dishwasher (20% water savings) shall be provided.

4. Energy Efficient Appliances

- a. Energy efficient Lights shall be provided.
- b. Occupant sensing lighting that dims to at least 50 percent when unoccupied shall be within the interior areas of warehouses and offices.
- c. Energy Star Commercial Refrigerators and Commercial Dishwashers shall be provided.

5. Energy Efficient Landscaping

- a. Only low water using plants shall be used.
- b. Weather based irrigation control systems combined with drip irrigation (demonstrate 20% reduced water) shall be used.
- c. Graywater (purple pipe) irrigation system shall be provided on site.

6. Energy Efficient Transportation

- a. A Car/vanpool program with preferred parking shall be provided within SUNSET CROSSROADS.
- b. Secure bicycle storage racks or bicycle lockers, and employee lockers will be provided within the industrial land uses

- c. Development shall provide reserved preferential parking spaces for car-share, carpool, and ultra-low or zero emission vehicles.
- d. Larger parking spaces will be provided that can accommodate vans used for ride-sharing programs and reserve them for vanpools and include adequate passenger waiting/loading areas.
- e. EV charging stations shall be installed in employee garages/parking areas.
- f. Adequate areas for on-site parking, on-site queuing, and truck check-in point will be provided.
- g. Development Project installed traffic signals shall be smart signals that can be synchronized and connected to an ITS system.
- h. Post signs clearly showing the designated entry and exit points from the public street to the designated onsite truck check-in and truck parking areas.
- i. Post signs indicating that all parking and maintenance of trucks must be conducted within the designated onsite areas and not within the surrounding community or public streets.
- j. Development Project street improvements will include sidewalks.
- k. Development Project will include a marketing strategy to promote the project site employer's CTR (Commute Trip Reduction) program. Information sharing and marketing promote and educate employees about their travel choices to the employment location beyond driving such as carpooling, taking transit, walking, and biking, thereby reducing VMT (Vehicle Miles Traveled). The following features (or similar alternatives) of the marketing strategy are essential for effectiveness.
 - a. Onsite or online commuter information services.
 - b. Employee transportation coordinators.
 - c. Onsite or online transit pass sales.

Development Project will provide tenant's employees material and online resources as a means to promote the commute trip reduction program. With proper implementation and 100 percent of the employees eligible, this design feature is expected to reduce VMT by 4 percent.

- I. Development Project will provide a ridesharing program and establish a permanent transportation management association with funding requirements for employers. Ridesharing encourages carpooled vehicle trips in place of single-occupied vehicle trips, thereby reducing the number of trips and VMT. Ridesharing must be promoted through a multifaceted approach. Examples include the following
 - a. Designating a certain percentage of desirable parking spaces for ridesharing vehicles.
 - b. Designating adequate passenger loading and unloading and waiting areas or ridesharing vehicles.
 - c. Providing an app or website for coordinating rides.

Development Project as designed, will provide carpool/vanpool/EV parking designated spaces in locations of easy and convenient accessibility to the Project building. As calculated for the Project, with proper implementation and 100 percent employees eligible, the Project is expected to reduce VMT by four percent.

DESIGN GUIDELINES

4

CHAPTER FOUR establishes the quality and character of the built environment through the design of architecture and landscaping for the master-planned development of SUNSET CROSSROADS.



- 4.1 PURPOSE AND INTENT
- 4.2 DESIGN THEME
- 4.3 INDUSTRIAL ARCHITECTURAL GUIDELINES
- 4.4 GENERAL COMMERCIAL ARCHITECTURAL GUIDELINES
- 4.5 SIGNAGE DESIGN GUIDELINES
- 4.6 LANDSCAPE DESIGN GUIDELINES

CHAPTER 4 | DESIGN GUIDELINES

4.1 PURPOSE AND INTENT

The Design Guidelines presented in this section establish the quality and character of the built environment for the master-planned development of SUNSET CROSSROADS. While the Design Guidelines provide direction, they are simultaneously intended to provide a certain level of flexibility to allow creative expression during the design of implementing development projects.

The Design Guidelines provide criteria for architecture, landscape design, signage, lighting, and energy efficiency.

SUNSET CROSSROADS' visual identity will be expressed primarily through building form, colors and materials, along with landscape, hardscape, and signage elements. The Design Guidelines contained herein are presented in a manner that ensures consistent, coherent architectural and landscape architecture expression across the Specific Plan area, while allowing for flexibility in response to the modern-day reality of continuously evolving building design.

The objectives of the Design Guidelines are:

- ❖ To describe the thematic elements and the construction quality expected for SUNSET CROSSROADS.
- ❖ To provide the City of Banning with assurance that SUNSET CROSSROADS will be developed in accordance with the quality and character described within this Specific Plan.
- ❖ To serve as a guide to developers, builders, engineers, architects, landscape architects, and other professionals involved with implementing development within SUNSET CROSSROADS, in order to achieve and maintain the desired design quality.
- ❖ To provide an aesthetic benchmark for the City of Banning to use in their review of future applications submitted within this Specific Plan within the Specific Plan.
- ❖ To provide a baseline for SUNSET CROSSROADS to convey a contemporary aesthetic theme and character while allowing flexibility for practical application and creative expression.
- ❖ To encourage the implementation of energy efficiency design features that can be implemented in the site planning, design, and construction phases of the Specific Plan to minimize waste deposited at landfills, decrease energy use and particularly fossil fuel consumption, and reduce water consumption.
- ❖ To ensure that the Specific Plan implements the intent of the City of Banning General Plan and applicable City Ordinances.

The Design Guidelines presented in this section apply to all development within SUNSET CROSSROADS, regardless of the land use category. All photographs, illustrations, and diagrams contained in these Design Guidelines serve as visual aids to convey the overall theme. Exact replication of the examples is neither required nor anticipated.

4.2 DESIGN THEME

The SUNSET CROSSROADS Specific Plan is a contemporary employment and commercial center containing General Commercial, Industrial, Open Space - Parks, and Open Space - Resource land uses. The design goal of SUNSET CROSSROADS is to achieve contemporary interpretations of historical styles rather than exact recreations. Architectural design creativity, attention to detail, and respect of the building's scale and massing along public roadways will be expected to be a level equal to or exceeding the General Commercial and Industrial uses within the City of Banning. The property's location provides the businesses located here with access to the regional transportation network, proximity to a work force, proximity to the Ports of LA and Long Beach, and visibility to passers-by traveling on Interstate 10. A cohesive design theme for SUNSET CROSSROADS is created for the General Commercial and Industrial land uses to maintain design continuity throughout the Specific Plan area.

The SUNSET CROSSROADS SPECIFIC PLAN establishes 47.9 acres of General Commercial designated area. Envisioned as a health and wellness-focused recreation, restaurant, and hospitality destination within the City of Banning, SUNSET CROSSROADS combines these uses within a carefully designed and cohesive architectural and landscape theme. The Specific Plan also establishes 392.0 acres of Industrial land uses, featuring a contemporary aesthetic, which provides architectural styling with attractive detailing, steel accents, a light-toned color palette, and timeless features. Design elements are included to reduce the visibility and intensity of the industrial activities, including walls, landscaping, and building design. Signs are modern, lighting is focused and directed, landscaping is colorful and drought-tolerant, and design features are applied that lower energy use demands of building operations.



Although provided for illustrative purposes only, the image above conceptually shows the design theme for General Commercial uses within SUNSET CROSSROADS.

4.3 INDUSTRIAL ARCHITECTURAL GUIDELINES

The architectural style of Industrial uses within SUNSET CROSSROADS emphasizes a contemporary interpretation of the traditional context with building massing over structural articulation. Buildings are characterized by simple and distinct cubic masses with interlocking volumes of wall planes, colors, and materials to create visual appeal, aesthetically pleasing proportions and strong shadow patterns. Colors, materials, and textures can be mixed to create additional visual interest.

Design elements are selected to be compatible in character, massing, and materials to promote a clean and contemporary feel. Individual creativity and identity are encouraged, but design integrity and compatibility must be maintained among all buildings and between Planning Areas, to reinforce a unified image within SUNSET CROSSROADS.

4.3.1 BUILDING FORM

Building "form" is one of the primary elements of architecture. Numerous design aspects, including shape, mass (size), scale, proportion, and articulation, are elements of a building's form. Building forms are especially important for building façades that are visible along the following view corridors:

- ❖ Building façades in Planning Areas 2, 3, and 4 that are visible from Sunset Avenue.
- ❖ Building façades in Planning Areas 2, 3, 4, 5, 8, 9, and 10 that are visible from the SLB Extension.
- ❖ Building façades in Planning Area 4 that are visible from Bobcat Road and the off-site residential to the south.
- ❖ Building façades in Planning Areas 6 and 7 that are visible from Interstate 10.
- ❖ Building façades in Planning Areas 7, 8, 9, and 10 that are visible from Highland Home Road, and the off-site residential community to the west.

The following guidelines are intended to ensure that structural development is visually consistent, aesthetically appealing, and inviting to the site's business owners and employees, their visitors and customers, as well as passing motorists. Note that building façades that orient inward to truck courts or service areas or that are not clearly visible from abutting public roads, or are not abutting publicly accessible viewing areas, are still encouraged to incorporate these Building Form guidelines.



Although provided for illustrative purposes only, the image above conceptually shows the Industrial building form of SUNSET CROSSROADS.

- (1) Use simple geometric shapes as the overall building form. Rectangular forms are encouraged to promote balance and visual interest. Avoid arbitrary, complicated building forms.
- (2) Long horizontal wall planes visible from a public street should include a combination of periodic changes in exterior building materials, color, decorative accents, and articulated features.
- (3) Modulation and variation of building masses between adjacent buildings visible from public streets is encouraged.
- (4) Pedestrian entrances to buildings (with the exception of service doors and emergency exit doors) should be obvious, using changes in massing, color, and/or building materials.
- (5) Pedestrian and ground-level building entries intended for visitor use should be recessed or covered by architectural projections, roofs, or arcades in order to provide shade and visual relief.
- (6) Architectural and trim detailing on building façades should be clean, simplistic, and not overly complicated.
- (7) Materials applied to any elevations should turn the corner of the building and continue to a logical termination point in relation to other architectural features or massing.

4.3.2 BUILDING MATERIALS, COLORS, AND TEXTURES

Building materials and colors play a key role in developing a clean, contemporary visual environment; therefore, the selected exterior materials, colors, and textures should complement one another throughout SUNSET CROSSROADS. Slight variations are encouraged to provide visual interest.

- (1) Appropriate primary exterior building materials include concrete and similar materials, as well as tilt-up panels. The primary materials should be accented by secondary materials including but not limited to natural or fabricated stone, fire resistant wood siding (horizontal or vertical), and metal.
- (2) Trim details may include metal finished in a consistent color, plaster, or concrete elements finished consistently with the building treatment. Use of overly extraneous "themed" detailing, like oversized or excessive foam cornice caps, foam molding and window detailing is discouraged.
- (3) Material changes should occur at intersecting planes, preferably at the inside corners of change of wall planes, or where architectural elements intersect.
- (4) Primary exterior building colors should be light and warm tones. Darker and/or more vibrant accent colors should be provided in focal point areas, such as around building entrances and near outdoor gathering spaces.
- (5) Bright primary colors, garish use of color and arbitrary patterns or stripes that will clash with the color palette are discouraged, except in signage logos.
- (6) Exposed downspouts, service doors and mechanical screen colors should be the same color as the adjacent wall.



Although provided for illustrative purposes only, the image above conceptually shows the Industrial building materials and colors of SUNSET CROSSROADS.

4.3.3 WINDOWS AND DOORS

The patterns of window and door openings should correspond with the overall rhythm of the building and should be consistent in form, pattern, and color within each Planning Area. Guidelines for windows and doors within SUNSET CROSSROADS are as follows:

- (1) When possible, the positioning of doors and windows on individual building façades should occur in a symmetrical and repetitive pattern to create continuity.
- (2) Window styles and trims should be consistent in form and color in each Planning Area. Window trims should be finished in a consistent color on each building.
- (3) Gold or unfinished/untreated metal window or door frames are prohibited. Clear silver anodized frames are allowed.



Although provided for illustrative purposes only, the image above conceptually shows the design of windows and doors for Industrial buildings within SUNSET CROSSROADS.

- (4) Glass should be clear or colored with subtle reflectiveness. Silver glass is prohibited.
- (5) Pedestrian entry doors to buildings should be clearly defined by features such as overhangs, awnings, and canopies or embellished with decorative framing treatments – including but not limited to accent trim. Dark and confined entries, flush doorways (except emergency exit and service doors), and tacked-on entry alcoves are discouraged.
- (6) When necessary, to meet Riverside County Fire Code Standards, no windows are required.

4.3.4 WALLS AND FENCES

The following guidelines for walls and fencing will ensure that these features complement the overall SUNSET CROSSROADS design theme, and are attractive from public viewing areas, scaled appropriately, durable, and integrated consistently within the Specific Plan area.

- (1) Freestanding walls and fences should not exceed a height of fourteen (14') feet, measured from the base of the wall/fence to the top of wall/fence. For walls and fences that exceed a height of fourteen (14') feet, modifications up to one (1') foot are considered a Minor Modification pursuant to Section 5.2.3.7. Modifications of more than one (1') foot may be approved by the Community Development Director pursuant to Section 5.2.3.9.
- (2) Landscaped berms may be used in combination with walls or fences. When this occurs, the height of the berm may be in addition to the wall or fence height.
- (3) Landscaping may be used for visual screening instead of walls and fences in locations where a solid physical barrier is not needed.
- (4) Walls and fences in public view should be built with attractive, durable materials including but not limited to tubular steel, vinyl, or coated chain-link.
- (5) Along public street frontages, long expanses of wall surfaces should be offset and/or architecturally treated to prevent monotony. Techniques to accomplish this may include, but are not limited to: openings, material changes, pilasters and posts, and staggered sections.
- (6) Wall and fencing materials should be compatible with other design elements of SUNSET CROSSROADS.



Although provided for illustrative purposes only, the image above conceptually shows the design of walls and fences around the perimeter of Industrial buildings within SUNSET CROSSROADS.

4.3.5 TRUCK COURTS AND LOADING DOCKS

(1) Loading doors, service docks, and equipment areas should be oriented or screened to reduce visibility from public roads within SUNSET CROSSROADS. Where these loading areas face toward and are within 100' of public roads (measured from edge of pavement to edge of public sidewalk), screening should be provided. Screening may be accomplished with any combination of solid walls, fences, or landscaping that are compatible with the architectural expression of the building.

(2) No loading or unloading activity is permitted to take place from public streets.

(3) Truck and service vehicle entries should be designed to provide clear and convenient access to truck courts and loading areas such that passenger vehicle, pedestrian, and bicycle circulation is not adversely affected by truck movements.

(4) Loading bays that are utilized by refrigerated trailers should have dock seals and be equipped with plug-in electrical outlets.

(5) Conduit should be installed in truck courts in logical locations that would allow for the future installation of charging stations for electric trucks, in anticipation of this technology becoming available in the lifetime of SUNSET CROSSROADS.

(6) Electrical rooms to hold sufficiently sized electrical panels should be installed to facilitate the future potential installation of electrical connections from the electrical room to dock doors and/or a separate designated location where diesel engine trucks and/or trailers for freezer/refrigerated warehouse space would park and connect to the electrical system.

Although provided for illustrative purposes only, the image below conceptually shows the design of truck courts and loading docks within SUNSET CROSSROADS.



4.3.6 GROUND OR WALL-MOUNTED EQUIPMENT

(1) Ground-mounted equipment, including but not limited to mechanical or electrical equipment, emergency generators, boilers, storage tanks, risers, and electrical conduits, should be screened when feasible from public viewing areas including adjacent public roads. Screening may be accomplished with solid walls, fences, or landscaping.

(2) Electrical equipment rooms should be located within the building envelope. Pop-outs or shed-like additions are discouraged.

(3) Wall-mounted items, such as roof ladders or electrical panels, should not be located on the building façade facing abutting public roads. Wall-mounted items should be screened or incorporated into the architectural elements of the building so as not to be visually apparent from the street or other public areas.



Although provided for illustrative purposes only, the image above conceptually shows the screening of equipment for Industrial buildings within SUNSET CROSSROADS.

4.3.7 ROOFTOP EQUIPMENT

- (1) Rooftop equipment, including but limited to mechanical equipment, electrical equipment, storage tanks, cellular telephone facilities, satellite dishes, vents, exhaust fans, smoke hatches, and mechanical ducts, should be screened by rooftop screens or parapet walls so as not to be visible from abutting public roads, and publicly accessible locations within SUNSET CROSSROADS.
- (2) Integrate rooftop screens (i.e., parapet walls) into the architecture of the main building. Wood finished rooftop screens are prohibited.
- (3) Design the roofs of Industrial buildings to support the future installation of solar panels.

4.3.8 OUTDOOR EMPLOYEE AMENITIES

- (1) Bicycle racks, lockers, and/or storage should be provided at each building or in a common area that serves multiple buildings to encourage non-vehicular circulation.
- (2) Industrial buildings may include patio break areas, pedestrian walkways, seating areas, and overhead structures so that workers do not have to travel off-site for outdoor enjoyment.

Although provided for illustrative purposes only, the image to the right conceptually shows the location of bike racks at entrances to Industrial buildings within SUNSET CROSSROADS.



4.3.9 OUTDOOR LIGHTING

Outdoor lighting of SUNSET CROSSROADS is an essential architectural component that provides aesthetic appeal, enhances safe pedestrian and vehicular circulation, and adds to security. Lighting within the public rights-of-way shall adhere to any applicable City of Banning outdoor lighting standards.

Lighting on private property in the Specific Plan should adhere to the following guidelines.

- (1) Minimize glare and "spill over" light onto public streets, open space, Interstate-10, and adjacent properties by using downward-directed lights and/or cutoff devices on outdoor lighting fixtures, including spotlights, floodlights, electrical reflectors, and other means of illumination for structures, parking, loading, unloading, and similar areas.
- (2) Select all lighting fixtures used in the Specific Plan area from the same – or complementary – family of fixtures with respect to design, materials, fixture color, and light color. Use of LED lighting is encouraged.
- (3) Lights should be unbreakable plastic, recessed, or otherwise designed to reduce the problems associated with damage and replacement of fixtures.
- (4) Neon and similar types of lighting are prohibited except as part of advertising signage within SUNSET CROSSROADS.



Although provided for illustrative purposes only, the image above conceptually shows the design and locations of lighting for Industrial buildings within SUNSET CROSSROADS.

- (5) Locate all electrical meter pedestals and light switch/control equipment in areas with minimum public visibility or screen them with appropriate plant materials.
- (6) Illuminate parking lots, loading dock areas, pedestrian walkways, building entrances, and public sidewalks to the level necessary for building operation and security reasons. Dimmers and motion detectors are permitted.
- (7) Along sidewalks and walkways, the use of low mounted fixtures (ground or bollard height) is encouraged.
- (8) Use exterior lights to accent entrances, plazas, activity areas, and special features.
- (9) To illuminate parking lots or parking structures and their pedestrian links that provide more than five parking spaces for use by the general public, provide a minimum coverage of one foot-candle of light with a maximum of eight foot-candles on the parking or walkway surface, unless otherwise approved by the City of Banning for visibility and security.
- (10) To illuminate aisles and passageways within a building complex, provide a maximum of one-half to one foot-candle of maintained lighting.
- (11) High Pressure Sodium (HPS) light fixtures are prohibited for site lighting.
- (12) All sign lighting should be consistent with the Signage Design Guidelines.

4.3.10 REFUSE CONTAINERS AND OTHER OUTDOOR STORAGE

- (1) Refuse enclosures and equipment should be easily accessed by service vehicles, with enclosures and equipment located within a building's facade or within a screened enclosure.
- (2) The enclosure's design should reflect the architectural style of adjacent buildings and use similar, high-quality materials.
- (3) Landscaping or trellis work should screen enclosures visible from a street or connecting walkway and should be permanently maintained.
- (4) Outdoor storage areas should be located away from the street, behind or to the side of buildings. In addition to locating storage areas away from streets, walls or permanent landscaping should be used to screen stored materials.

4.3.11 SUPPLEMENTAL GUIDELINES FOR INDUSTRIAL USES

This section sets forth additional guidelines that address the design of building sites and considerations for all uses permitted within the Industrial land uses (Planning Areas 2 through 10).



Although provided for illustrative purposes only, the image above conceptually shows the design of Industrial buildings within SUNSET CROSSROADS.

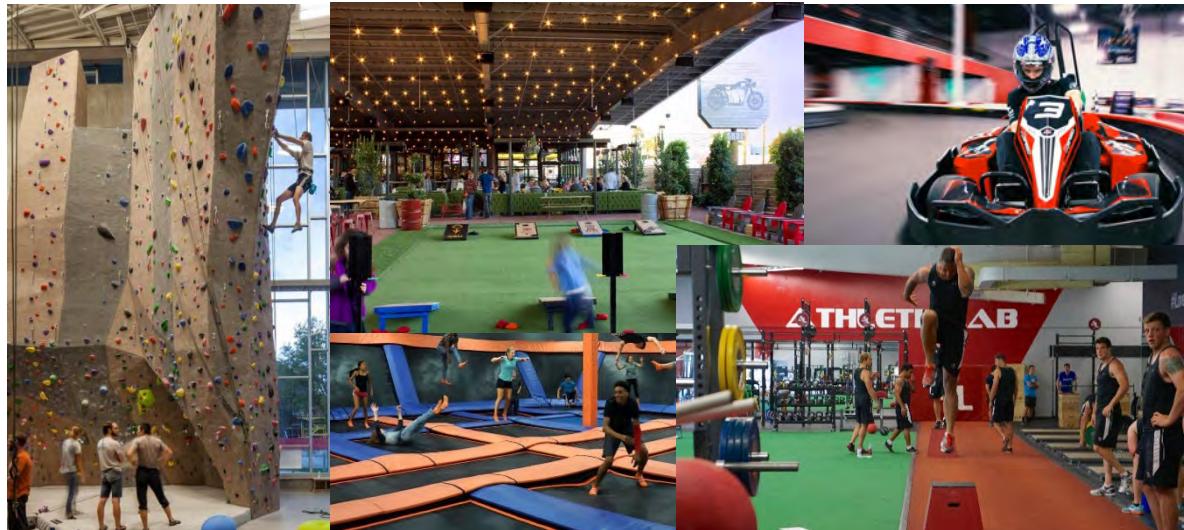
- (1) Locate the office portion of warehouse buildings at the corner(s) of the building. For buildings taking access from Sunset Avenue, Lincoln Street, and the SLB Extension, orient the office toward these roads to provide visual interest from the public roadway.
- (2) Orient and screen elements such as outdoor storage, ancillary fueling, ancillary services, trash enclosure areas in ways that minimize their visibility from Sunset Avenue, the SLB Extension, Highland Home Road, Bobcat Road, and off-site residential uses.
- (3) Site design should specifically address the needs of pick-up, delivery, and service vehicles associated with Industrial uses.
 - a. Design interior driveways and drive aisles to provide adequate stacking and prevent queuing of vehicles on public streets.
 - b. Locate and design service entrances so they do not interfere with owner/tenant/customer access.
 - c. Design loading areas to provide for tractor trailer backing and maneuvering on-site and not from a public street.
 - d. Provide appropriate on-site service vehicle parking/turnouts in an efficient, non-obtrusive location appropriate to the scale and needs of the development.
 - e. Vehicle loading/unloading when parked, should not impede normal traffic flow.
- (4) Architectural design of Industrial buildings may be utilitarian in form; however, the design should complement the SUNSET CROSSROADS overall design theme.
- (5) Textured forms, reveals, or scoring on concrete tilt-up panels is recommended for visual relief and to create a base and cornice expression; however, smooth panels with color variation may be used in lieu of textured finishes.
- (6) Use primary roof forms that are flat or gently sloping. The ridge line elevation of the primary roof form should not exceed the parapet wall. Change of parapet height is strongly encouraged.
- (7) Avoid the use of arched gable, hip and shed roof forms as a primary roof form. These roof forms may be used as a secondary/accent roof form.
- (8) With the exception of solar panels, screen or obscure all rooftop mounted equipment, where required, from public view using landscaping or materials complementary to those used on the main structure.

4.4 GENERAL COMMERCIAL ARCHITECTURAL GUIDELINES

The goal of the General Commercial Architectural Guidelines is to promote visual compatibility and individuality within SUNSET CROSSROADS. These guidelines do not require rigid adherence to style descriptions but are intended to assist in establishing the design direction and the quality of the end product. Achieving variety and diversity in the General Commercial architecture is a key objective for SUNSET CROSSROADS. The intent of the Design Guidelines is to provide a framework for development over time, while maintaining maximum flexibility and thematic consistency to accommodate a wide variety of uses. For inspiration, SUNSET CROSSROADS has embraced compatible architectural styles that have historical precedence in Southern California and throughout the world and are capable of contemporary interpretation and variation in a modern master planned environment. The intent is to avoid monotonous architecture, while encouraging the application of detail and materials authentic to the spirit and character of the Specific Plan.



Although provided for illustrative purposes only, the image above conceptually shows the design of General Commercial buildings within SUNSET CROSSROADS.



Although provided for illustrative purposes only, the images above conceptually show the anticipated recreational and wellness-based uses within the General Commercial areas of SUNSET CROSSROADS.

4.4.1 DESIGN PRINCIPLES

- (1) Site planning design principles are not intended to restrict taste or preference but are designed to avoid harsh contrasts and to foster harmony of development. For example, within parking lots, all ingress and egress locations should be designed to reduce impacts on the circulation system.
- (2) Ground floors of buildings adjacent to internal streets should be placed adjacent to the street edge to encourage pedestrian activity.
- (3) Free-standing buildings are encouraged to be grouped around a common focal point or design feature.
- (4) Open areas should be large enough to be usable, but not so large as to appear empty.
- (5) Major tenants or individual buildings at key locations should incorporate public plazas at focal points and at activity centers. Use of park areas and water elements in selected areas is encouraged.
- (6) Plazas and courtyards are encouraged to be used as transition areas between public and private spaces.
- (7) Continuity should be maintained through the use of unified or complementary pedestrian amenities, landscaping and similar design features.
- (8) Alternative paving or other design features should be used to provide for separation of pedestrian and vehicular circulation within the same right-of-way.

4.4.2 BUILDING PLOTTING CONCEPTS

- (1) Buildings should be oriented to take best advantage of arterial visibility.
- (2) When planning the interior vehicular circulation and parking, a hierarchy should be developed to help with traffic flow.
- (3) Building masses should be articulated to avoid long straight building façades and create enclosed courtyards and pedestrian spaces where possible.

4.4.3 STREETSCENE

- (1) To allow for comfortable pedestrian spaces, the Specific Plan should provide adequate separation between parking and storefronts.
- (2) The distance along the face of the building should be varied to provide visual interest.
- (3) Loading zones should be site planned to avoid exposure to adjacent public streets.
- (4) Exterior storage areas and trash enclosures should be planned to minimize the exposure of such elements.
- (5) Parking areas should be planned to allow for landscaping or low, decorative walls or a combination thereof.
- (6) Entries to service and delivery areas should be separated from parking areas where possible.



Although provided for illustrative purposes only, the image above conceptually shows the streetscene of General Commercial uses within SUNSET CROSSROADS.

4.4.4 BUILDING DESIGN, MASS, AND SCALE

- (1) Architecture should be comprised of simple and distinct masses. Masses should be expressed with clean, sharp edges and corners. A simple hierarchy should be established among the various masses.
- (2) Commercial buildings should be designed as a complementary addition to the overall theme established throughout SUNSET CROSSROADS.
- (3) Dramatic departures in form, scale, and style should not be permitted.
- (4) Buildings should be designed to create smooth transitions in scale through the use of low and mid-rise building forms or through the use of terraced elevations.
- (5) Long, uniform façades should be avoided by creating visual interest through the use of courtyards, varied building setbacks, arcades, windows and towers.
- (6) Buildings should be designed to locate high activity uses, such as restaurants and entertainment facilities, adjacent to major pedestrian ways.
- (7) Outdoor uses are strongly encouraged along pedestrian ways, as well as providing the highest level of detail and interest at ground level.
- (8) Canopies, awnings, and trellises are encouraged to define pedestrian ways and to offer protection from the elements.
- (9) Architectural elements and accessories should be provided on the building mass, including but not limited to, arcades, balconies, towers, colorful banners, and decorative lighting features.
- (10) Roof lines should be articulated with shorter elements to reduce building mass. Occasional sloped roof forms should be introduced over special areas and special functions to create visual interest.



Although provided for illustrative purposes only, the image above conceptually shows the building design of General Commercial uses within SUNSET CROSSROADS.

4.4.5 BUILDING MATERIALS AND COLORS

- (1) The commercial area is an integral part of SUNSET CROSSROADS, and the materials and colors should be compatible with the surrounding development. Color is intended to act as a primary theme-conveying element and will be reflective of the architectural style.
- (2) Building materials should be durable and convey a sense of permanence. The textural and material quality of building's facade is important. Traditional materials including stone, brick, concrete, precast concrete, metal, block,

and stucco should be used creatively to provide a sense of permanence. Combined with architectural details, landscape, and hardscape, the buildings can create a sense of connectivity to the pedestrian-oriented streetscene.

- (3) Exterior building materials should be of natural character that would reflect the rural character of the surrounding environment. Contemporary materials are acceptable if they are compatible with the surroundings and are used for creating interest.
- (4) Color is intended as a primary theme element of the community consistent with indigenous elements of the environment. Accents are encouraged which are lighter or darker to highlight the character of the structure. Bright and non-earth tone colors are not encouraged except as accents.

Although provided for illustrative purposes only, the image below conceptually shows a mixture of building materials and colors used on the General Commercial buildings within SUNSET CROSSROADS.

Building Material Palette



Building Color Palette



Paint Color Palette



4.4.6 ROOF FORM AND MATERIALS

- (1) Principal roof forms should be gable or hip with pitches.
- (2) All roof material should ensure continuity in texture, color and character to the architectural styles.
- (3) The design should create interesting building masses by varying rooflines and by maximizing offsets to roof planes where possible. Combining single-story elements with two-story elements is encouraged.
- (4) Flat roofs with parapet walls are acceptable but should be used in combination with simple pitched gable, hip or shed roof forms.
- (5) Mechanical equipment, other than solar panels, on roofs should be screened from view of I-10, public roadways, and adjacent residential homes, to the extent practical, and should consist of materials consistent with those of the building.

4.4.7 WINDOWS AND DOORS

- (1) Recessed doors, windows and wall openings emphasizing massive wall thickness are characteristic elements of the allowed architectural styles. Fully recessed door and window openings are encouraged as well as embellished framing treatments of both to add articulation to the wall surface.
- (2) Consideration should be taken to relate interior building design to pedestrian spaces through generous use of glazing in doors and windows.
- (3) Other enhancements which are encouraged are: recessed windows and doors to create shade and wall articulation, arched windows and doorways, decorative treatments, accent trim or tile at doorways, banded windows to emphasize the horizontal, glazing which follows roof pitch, canvas awnings with complementary accent colors, and wrought iron accents.
- (4) Silver or gold window or door frames, reflective glass or awnings, and metal awnings are all discouraged.

4.4.8 PARKING/VEHICULAR CIRCULATION

- (1) Parking lots should generally be placed away from the street, preferably behind buildings.
- (2) Adequate areas for maneuvering, loading, and emergency vehicle access should be accommodated on site.
- (3) Parking areas should be arranged to minimize conflicts with commercial loading activities
- (4) Large parking lot areas should be divided into a series of smaller connected lots separated by additional landscaping and buildings.
- (5) Commercial loading and parking should generally be located to the side and rear of buildings with the exception of handicap and short-term parking lots which may be sited between the street and building entrances.

4.4.9 SERVICE, LOAD, AND EQUIPMENT AREAS

- (1) Storage for supplies, merchandise, and similar materials should be prohibited on the roofs of any buildings.
- (2) Service, loading, and storage areas should be separated from pedestrian and private automobile circulation whenever practical to do so.
- (3) Service areas, loading docks, and equipment areas should be screened from views either by locating these uses within a building or by screening them with landscaping, walls, fences, or other architectural treatments.
- (4) For screening of loading, service, and/or storage areas to be effective, a minimum treatment height of six (6) feet should be used.

4.4.10 OUTDOOR LIGHTING

Outdoor lighting of SUNSET CROSSROADS is an essential architectural component that provides aesthetic appeal, enhances safe pedestrian and vehicular circulation, and adds to security. Lighting within the public rights-of-way shall adhere to any applicable City of Banning outdoor lighting standards.

Lighting on private property in the Specific Plan shall comply with the Development Standards in this Specific Plan and should adhere to the following guidelines.

- (1) All outdoor lighting, including spotlights, floodlights, electrical reflectors, and other means of illumination for structures, landscaping, parking, loading, unloading, and similar areas should be focused, directed, and arranged to prevent glare and illumination on streets or adjoining property.

- (2) Low intensity, energy conserving night-lighting is preferred.
- (3) No freestanding lighting fixtures should exceed 30 feet in height.
- (4) Service area lighting should be contained within the service yard boundaries and enclosure walls. No light spillover should occur outside the service area.
- (5) No specific design is prescribed and variations in treatments should be allowed; however, light fixtures and standards should be coordinated along public rights-of-way and within the commercial area.
- (6) Parking lot lighting should provide adequate illumination for the safety of visitors while minimizing glare into adjacent property and uses.
- (7) Energy conservation, safety, and security should be emphasized during the designing and siting of project lighting.
- (8) Light standards should blend architecturally with buildings, pedestrian areas, other hardscape elements, and street furniture. Non-decorative wall-pacs and light standards should not be permitted.
- (9) Parking areas should use lighting standards and fixtures that are consistent with and a continuation of the character of the development and structures.
- (10) Exposed neon should not be permitted. All neon tubes should be hidden from view.



Although provided for illustrative purposes only, the image above conceptually shows the design of lighting within the General Commercial areas of SUNSET CROSSROADS.

4.4.11 REFUSE CONTAINERS AND OTHER OUTDOOR STORAGE

- (1) Refuse enclosures and equipment should be easily accessed by service vehicles, with enclosures and equipment located within a building's facade or within a screened enclosure.
- (2) The enclosure's design should reflect the architectural style of adjacent buildings and use similar, high-quality materials.
- (3) Landscaping or trellis work should screen enclosures visible from a street or connecting walkway and should be permanently maintained.
- (4) Shopping cart storage areas should be integrated within the initial building and site design. Large freestanding enclosures or unscreened "cart corrals" are not encouraged but should be allowed if they are constructed as a permanent structure and architecturally consistent and include landscape treatment such as vines or planters.
- (5) Outdoor storage areas should be located away from the street, behind or to the side of buildings. In addition to locating storage areas away from streets, walls or permanent landscaping should be used to screen stored materials.

4.4.12 SUPPLEMENTAL GUIDELINES FOR GENERAL COMMERCIAL

This section sets forth guidelines that address the design of building sites and considerations unique to the General Commercial uses within SUNSET CROSSROADS (Planning Area 1).

- (1) Orient buildings to have tenant visibility from internal streets.
- (2) Free-standing architectural treatment may be used in front of building walls to create layering.

- (3) Use simple building forms and maximize the play of light on mass and void to provide strong contrasts. Blank walls should be carefully balanced with glass and wall areas.
- (4) Position lower building masses, signage, doors, light fixtures, and/or landscape planters adjacent to pedestrian entries, particularly entries that may face internal streets to create human-scaled development at these locations.
- (5) Orient primary building entrances away from off-street parking areas.
- (6) Provide well-defined pedestrian connections from parking areas to building entrances.
- (7) The use of towers and well-proportioned building elements (arcades, colonnades, recesses, etc.) are encouraged to define entries and create pedestrian scale.
- (8) Design architectural details (rafter tails, gabled towers, column detail, stone base, etc.) in a modern contemporary aesthetic.
- (9) Locate service entrances such that they do not interfere with owner/tenant/customer access.
- (10) Design loading areas to provide for backing and maneuvering on-site.
- (11) Provide appropriate on-site service vehicle parking/turnouts in an efficient, non-obtrusive location suitable to the scale and needs of the development. Service and delivery/loading areas should be separated from customer parking areas and pedestrian circulation areas (walkways, pathways, etc.).
- (12) Where feasible, provide clearly delineated pedestrian paths from the sidewalks on Sunset Avenue and Lincoln Street to hospitality, restaurant and recreation land uses constructed within the General Commercial areas.
- (13) Position bicycle parking areas near the main entrances of buildings.
- (14) Design roofs for functionality and to complement the overall architectural design of the building. Incorporate vertical building plane breaks, through changes in building/ridge height, or other accent roof forms to create visual interest. On flat roofs, the ridge line elevation of the primary roof form should not exceed the parapet wall.
- (15) When parapet walls are used, incorporate side/rear elevation returns to preclude an unfinished, "false front" appearance.
- (16) Screen all rooftop mounted equipment (including solar panels to the extent practical) from public view using materials complementary to those used on the main structure.

Although provided for illustrative purposes only, the image below conceptually shows the design of SUNSET CROSSROADS.



4.5 SIGNAGE DESIGN GUIDELINES

Signage within the Specific Plan area serves a variety of purposes. Signs will identify SUNSET CROSSROADS and its building occupants and ensure the efficient circulation of vehicle traffic within the site by identifying vehicular entry points and directing vehicles to their on-site destinations. Also, signage will enhance the pedestrian experience through the design of way finding components: directories, directional signage and destination identifiers.

As such, clear, concise, and easy-to-understand signage that is also visually appealing is vitally important for positive worker and visitor experiences at SUNSET CROSSROADS.

Although provided for illustrative purposes only, the image below conceptually shows the design of signage within SUNSET CROSSROADS.



A separate Specific Plan Sign Program including Signage Design Guidelines shall be prepared and approved by the City of Banning prior to the issuance of the first Occupancy Permit within the Specific Plan. Changes to any sign requirements or regulations contained in the Specific Plan following approval of the Specific Plan are subject to a "Minor Modification" and shall be administratively reviewed and approved by the Community Development Director.

1. The Monumentation and Signs within Section 4.6.4, *Monumentation*, are provided as conceptual and illustrative purposes only, and may be modified as part of the Sign Program.
2. The following design considerations in this section should be reflected in the Sign Program:
 - (a) The Sign Program should provide adequate and appropriate project, pylon, street, building, commercial tenant identification, trail, path, and wayfinding signage for the anticipated variety of building sizes, designs, and uses to ensure that all project signage is designed with a single vision and theme, generally outlined below.
 - (b) All building signage should be in scale with and in proportion to, the primary building facades so that the signage is not 'overpowering' and does not dominate the overall appearance.
 - (c) The design of the signage should reflect the type of business and/or uses through its design, material, shape and graphic form. Signage is encouraged to use natural materials where possible.

- (d) All signs should be so oriented as to preclude hazardous obstructions to person and/or vision of pedestrians and/or vehicle operators.
- (e) Building occupant identification signage should be in keeping with the character established for SUNSET CROSSROADS with variations allowed to accommodate individual user identities/corporate branding standards.
- (f) The method of attaching the signage to the building should be integrated into the Sign Program.
- (g) Signs should not cover up windows or important architectural features.
- (h) All conductors, conduits, tubing, crossovers, transformers, and other equipment for the illumination of signs should be concealed and/or incorporated into the building architecture or within the sign structure.
- (i) Exposed wiring, raceways, cords, plugs, or the like will not be permitted.
- (j) Direction signs may be located at any vehicular or pedestrian decision point.
- (k) Vehicular direction signs should clearly direct to destination anchors within SUNSET CROSSROADS, such as on-site parking areas, and service routes.
- (l) Vehicular direction signs should be consistent in size, shape, and design throughout SUNSET CROSSROADS.
- (m) Typography on vehicular direction signs should be legible and have enough contrast to be read from an appropriate windshield viewing distance.

- (n) Vehicular direction signs should incorporate reflective vinyl copy for night-time illumination.
- (o) Freestanding signs, pylon signs, and Freeway Oriented Freestanding Signs are permitted and may consist of the project's name, project's logo, tenant logos, and/or tenant text.

4.6 LANDSCAPE DESIGN GUIDELINES

These Landscape Design Guidelines establish landscape principles and standards that apply to all Planning Areas within SUNSET CROSSROADS to ensure that plant materials, hardscapes, entries, monuments, streetscapes and other amenities are compatible with the overall design theme, and that all implementing development projects are united under a common design theme. These Landscape Design Guidelines, when taken with the companion Architectural Design Guidelines provided herein, establish an identity for SUNSET CROSSROADS that is contemporary and visually appealing and compatible with the adjacent uses.

Although a great deal of detailed design information is presented herein, these Guidelines are not intended to establish a set of rigid landscaping requirements for SUNSET CROSSROADS. It is recognized that, at times, there will be a need to adapt these Guidelines to meet certain parcel-specific or user-identity requirements. As such, these Landscape Guidelines are intended to be flexible and are subject to modification over time. However, it is critical to the SUNSET CROSSROADS long-term design integrity that any deviations from these Landscape Guidelines are in keeping with the spirit of the core elements of the overall theme described herein, to ensure a cohesive and unified landscape concept across SUNSET CROSSROADS.

The Landscape Design Guidelines serve the dual purpose of adding year-round visual appeal while being sensitive to the environment, and the Southern California climate, by using drought-tolerant materials. Landscaping occurs throughout SUNSET CROSSROADS, but most prominently at street corners, along exterior roadways to create buffers to adjoining uses, and at building entrances and in passenger car parking lots. Monumentation featuring colorful accent trees, shrubs, and groundcover provided at entries welcome employees and visitors to SUNSET CROSSROADS occur at the corners of entrances.

Streetscape landscaping presents a combination of evergreen and deciduous trees, low shrubs, and masses of groundcovers to create a visually pleasing experience for pedestrians and passing motorists. As identified on Figure 4-1, *Conceptual Master Landscape Plan*, the SUNSET CROSSROADS thematic identity is reinforced by the landscape design of interfaces, monumentation, streetscapes, and pedestrian pathways and promenades. Furthermore, the recommended Plant Palette, community elements, and hardscape materials work in concert to reinforce and emphasize the SUNSET CROSSROADS landscape theme.

4.6.1 PLANT PALETTE

The Plant Palette for SUNSET CROSSROADS was selected to complement and enhance the setting of the site, while ensuring the conservation of the site's natural vegetation and habitats. The Plant Palette was selected to complement SUNSET CROSSROADS' architectural styles and design elements (hardscape, monumentation, walls and fences, etc.). Table 4-1, *Plant Palette*, provides a base plant palette which includes a list for SUNSET CROSSROADS' landscape design. Alternative plant species may be used in lieu of species listed in Table 4-1, provided the alternative plants are drought-tolerant and complement the SUNSET CROSSROADS design theme.

Three (3) Plant Palette Categories are used to establish and differentiate areas of SUNSET CROSSROADS and are described below and identified in Figure 4-2, *Conceptual Landscape Zones*.

❖ Zone A - Entrance Planting

Zone A consists of plant material designed to create a strong opening statement. These plants will be arranged for viewing from public roadways such as Sunset Avenue, Lincoln Street, and the SLB Extension, as well as for viewing from I-10.

❖ Zone B – Native California Palette

Zone B will make up the majority of the aesthetic landscape surrounding the entrances and fronts of the buildings within the General Commercial and Industrial areas. Zone B creates a satisfying backdrop to any visitor's stay, and contains plants that are native, drought tolerant, and perform admirably in Banning's climate.

❖ Zone C – Industrial Screen Planting

Zone C will be made up of tall, large, evergreen trees and thick groundcover along the perimeters of SUNSET CROSSROADS. Zone C is also intended to screen industrial buildings and any truck traffic passing through.

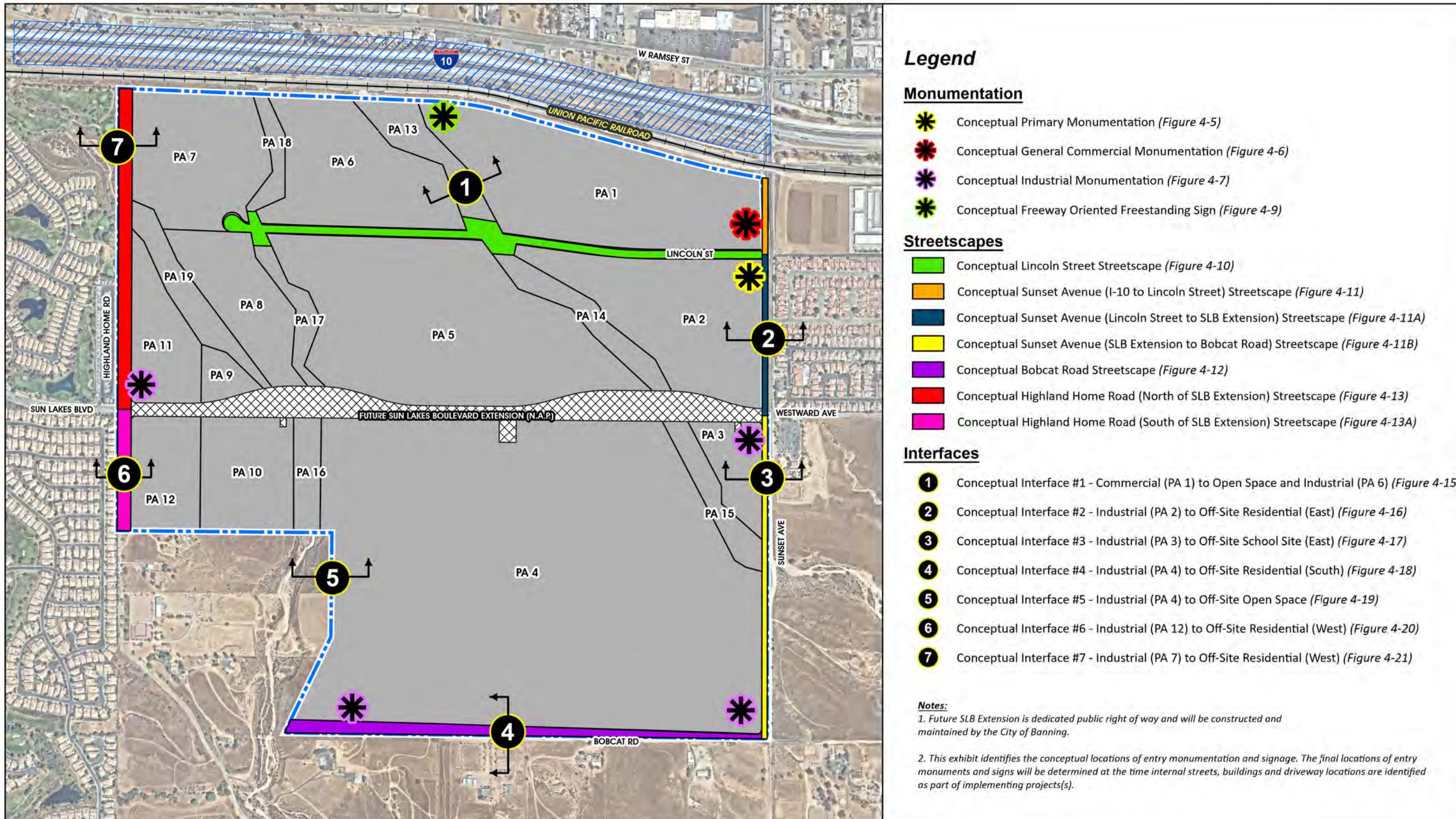
Photos of selected plants from the Plant Palette can be found in Figure 4-3, *Conceptual Plant Palette Imagery*.

Table 4-1 Plant Palette

| TREES | | | | | | | | | | | | |
|----------------------------|------------------------|---------|---------|--------------|-----------|--------|--------|--------|------------|----------|----------|----------|
| Botanical Name | Common Name | Entries | Streets | Parking Lots | Screening | Accent | Basins | Slopes | Size | Spacing | Min Size | Max Size |
| Acacia smallii | Acacia | * | | | | * | | * | 20' x 30' | 30' O.C. | 15 gal | 48" box |
| Acacia stenophylla | Shoestring Acacia | | | | * | | | | 40' x 30' | 30' O.C. | 15 gal | 48" box |
| Cercidium 'Desert Museum' | Blue Palo Verde | | | | | * | | * | 25' x 25' | 25' O.C. | 15 gal | 48" box |
| Chilopis linearis | Desert Willow | | | | | * | * | * | 20' x 15' | 15' O.C. | 15 gal | 48" box |
| Chitalpa tashkentensis | Chitalpa | * | * | | | * | | * | 30' x 30' | 30' O.C. | 15 gal | 48" box |
| Heteromeles arbutifolia | Toyon | | * | * | * | | | * | 8' x 5' | 5' O.C. | 15 gal | 48" box |
| Koelreuteria bipinnata | Chinese Flame Tree | | * | * | * | | | | 40' x 30' | 30' O.C. | 15 gal | 48" box |
| Magnolia g 'Samuel Sommer' | Magnolia | | * | * | * | | | | 40' x 30' | 30' O.C. | 15 gal | 48" box |
| Olea wilsoni | Wilson Fruitless Olive | * | | | | * | | | 30' x 30' | 30' O.C. | 15 gal | 48" box |
| Pinus eldarica | Afghan Pine | * | * | | * | | | | 60' x 40' | 40' O.C. | 15 gal | 48" box |
| Pistacia Chinensis | Chinese Pistache | | * | * | * | | | * | 35' x 30' | 30' O.C. | 15 gal | 48" box |
| Platanus acerifolia | London Plane | * | * | | * | * | | | 100' x 75' | 75' O.C. | 15 gal | 48" box |
| Platanus racemosa | California Sycamore | * | * | | | * | | * | 80' x 50' | 50' O.C. | 15 gal | 48" box |
| Quercus agrifolia | Coast Live Oak | | * | | | | | | 70' x 70' | 70' O.C. | 15 gal | 48" box |
| Quercus virginiana | Southern Live oak | | * | * | | | * | * | 50' x 80' | 80' O.C. | 15 gal | 48" box |
| Rhus lancea | African Sumac | * | * | * | | * | | * | 30' x 40' | 40' O.C. | 15 gal | 48" box |
| Salix lesilepis | Arroyo Willow | * | * | | | * | | | 30' x 30' | 30' O.C. | 15 gal | 48" box |
| Tristaniopsis conferta | Brisbane Box Tree | | * | | * | | | * | 20' x 20' | 20' O.C. | 15 gal | 48" box |
| SHRUBS | | | | | | | | | | | | |
| Botanical Name | Common Name | Entries | Streets | Parking Lots | Screening | Accent | Basins | Slopes | Size | Spacing | Min Size | Max Size |

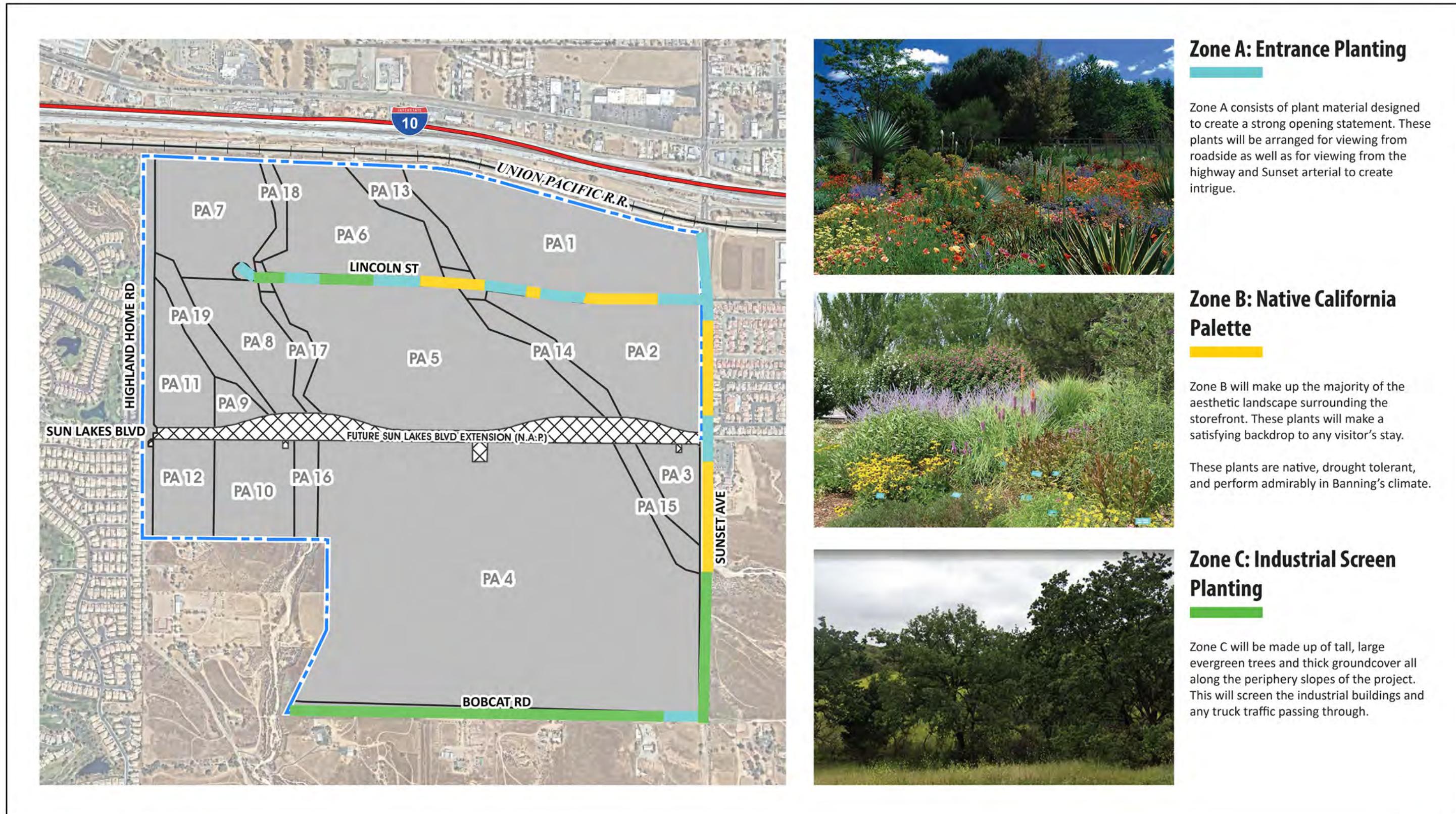
| Ligustrum j. 'Texanum' | Texas Privet | * | * | * | | | | | 6' x 3' | 3' O.C. | 1 gal | 5 gal |
|------------------------------------|-------------------------|---------|---------|--------------|-----------|--------|--------|--------|----------|----------|----------|----------|
| Acca sellowiana | Pineapple Guava | * | | * | * | | | | 6' x 3' | 3' O.C. | 1 gal | 5 gal |
| Arbutus unedo | Strawberry Tree | * | * | * | | | | | 8' x 3' | 3' O.C. | 1 gal | 5 gal |
| Calliandra californica | Baja Fairy Duster | | | * | * | | * | * | 4' x 4' | 4' O.C. | 1 gal | 5 gal |
| Lavandula 'Goodwin Creek Grey' | Lavender | * | * | * | | * | | | 3' x 3' | 3' O.C. | 1 gal | 5 gal |
| Callistemon 'Little John' | Dwarf Bottlebrush | | | | * | | * | * | 3' x 3' | 3' O.C. | 1 gal | 5 gal |
| Leucophyllum spp. | Texas Ranger | | | | * | * | * | * | 4' x 4' | 4' O.C. | 1 gal | 5 gal |
| Rhamnus californica | Coffeeberry | | | | * | * | * | * | 6' x 4' | 4' O.C. | 1 gal | 5 gal |
| Salvia greggii | Autumn Sage | * | * | * | * | | | | 3' x 3' | 3' O.C. | 1 gal | 5 gal |
| Senna artemisioides | Feathery Cassia | | | * | * | | | | 4' x 4' | 4' O.C. | 1 gal | 5 gal |
| Westringia spp. | Coast Rosemary | * | | | | * | | | 5' x 5' | 5' O.C. | 1 gal | 5 gal |
| ACCENTS | | | | | | | | | | | | |
| Botanical Name | Common Name | Entries | Streets | Parking Lots | Screening | Accent | Basins | Slopes | Size | Spacing | Min Size | Max Size |
| Agave spp. | Agave | | | | | * | | | 3' x 5' | 3' O.C. | 1 gal | 5 gal |
| Aloe spp. | Aloe | | | | | * | | | 2' x 3' | 3' O.C. | 1 gal | 5 gal |
| Cactus spp. | Cactus | | | | | * | | | 2' x 3' | 3' O.C. | 1 gal | 5 gal |
| Hesperaloe parviflora | Red Yucca | | | | | * | | | 5' x 3' | 3' O.C. | 1 gal | 5 gal |
| Yucca spp. | Yucca | | | | | * | | | 3' x 3' | 3' O.C. | 1 gal | 5 gal |
| GROUNDCOVER | | | | | | | | | | | | |
| Botanical Name | Common Name | Entries | Streets | Parking Lots | Screening | Accent | Basins | Slopes | Size | Spacing | Min Size | Max Size |
| Acacia redolens 'Low Boy' | Dwarf Acacia | | | | | | * | * | 2' x 8' | 8' O.C. | Flats | 5 gal |
| Baccharis p. 'Pigeon Point' | Dwarf Coyote Bush | | * | * | * | | | | 2' x 6' | 6' O.C. | Flats | 5 gal |
| Carex pansa | California Meadow Sedge | | | * | | | | | 1' x 12" | 12" O.C. | Flats | 5 gal |
| Carissa m. 'Green Carpet' | African Daisy | | * | * | | | | | 2' x 3' | 3' O.C. | Flats | 5 gal |
| Festuca mairei | Atlas Fescue | * | | * | | | * | * | 2' x 2' | 2' O.C. | Flats | 5 gal |
| Hemerocallis hybridus-Yellow | Yellow Day Lily | * | * | * | | | | | 2' x 2' | 2' O.C. | Flats | 5 gal |
| Lantana 'Gold Mound' | Yellow Lantana | * | | | | | | | 2' x 3' | 3' O.C. | Flats | 5 gal |
| Liriope gigantea | Big Blue Lily Turf | * | | | | | | | 2' x 3' | 2' O.C. | Flats | 5 gal |
| Lonicera j. 'Halliana' | Hall's Honeysuckle | * | | | | | | | 2' x 4' | 4' O.C. | Flats | 5 gal |
| Myoporum parvifolium | Mypoporum | * | | | | | | | 2' x 3' | 3' O.C. | Flats | 5 gal |
| Rose 'Flower Carpet'-Red | Red Flower Carpet Rose | * | * | | | | | | 2' x 30" | 30" O.C. | Flats | 5 gal |
| Rosmarinus o. 'Huntington Carpet' | Rosemary | | * | | | | | | 2' x 4' | 4' O.C. | Flats | 5 gal |
| Sesleria autumnalis | Moor Grass | | * | | * | | | | 2' x 18" | 18" O.C. | Flats | 5 gal |
| Trachelospermum jasminoides | Star Jasmine | * | * | | | | | | 2' x 2' | 2' O.C. | Flats | 5 gal |
| Tulbaghia violacea | Society Garlic | * | | | | | | | 2' x 2' | 2' O.C. | Flats | 5 gal |
| Basins/Swales-Applied by Hydroseed | | Bottom | Slopes | | | | | | | | | |
| Achillea millefolium | Yarrow | * | | | | | | | | | | |

| | | | | | | | | |
|---------------------------------|----------------------|---|---|--|--|--|--|--|
| <i>Eschscholzia californica</i> | California Poppy | * | | | | | | |
| <i>Juncus bufonius</i> | Toad Rush | * | | | | | | |
| <i>Leymus tricoides</i> Rio | Creeping Wild Rye | * | | | | | | |
| <i>Deschampsia cespitosa</i> | Tuft Hair Grass | * | | | | | | |
| <i>Festuca rubra</i> 'Molte' | Red Fescue | * | | | | | | |
| <i>Hordium brachyantherum</i> | Meadow Barley | * | | | | | | |
| <i>Muhlenbergia regens</i> | Deer Grass | * | | | | | | |
| <i>Muhlenbergia microsperma</i> | Little Seed Muhly | * | | | | | | |
| <i>Hordium depressum</i> | Low Barley | * | | | | | | |
| <i>Aristida purpuria</i> | Purple three-awn | | * | | | | | |
| <i>Artemisia californica</i> | California Sagebrush | | * | | | | | |
| <i>Eriogonum fasciculatum</i> | Buckwheat | | * | | | | | |
| <i>Mimulus aurantiacus</i> | Monkey flower | | * | | | | | |
| <i>Encelia farinosa</i> | Brittlebush | | * | | | | | |
| <i>Salvia alpinia</i> | White Sage | | * | | | | | |
| <i>Salvia mellifera</i> | Black Sage | | * | | | | | |
| <i>Acmonia glaber</i> | Deer Weed | | * | | | | | |
| <i>Eschscholzia californica</i> | California Poppy | | * | | | | | |
| <i>Lupinus truncatus</i> | Lupine | | * | | | | | |
| <i>Salvia columbariae</i> | Chia Sage | | * | | | | | |
| <i>Plantago erecta</i> | California Plaatin | | * | | | | | |
| <i>Festuca microstachys</i> | Small Fescue | | * | | | | | |



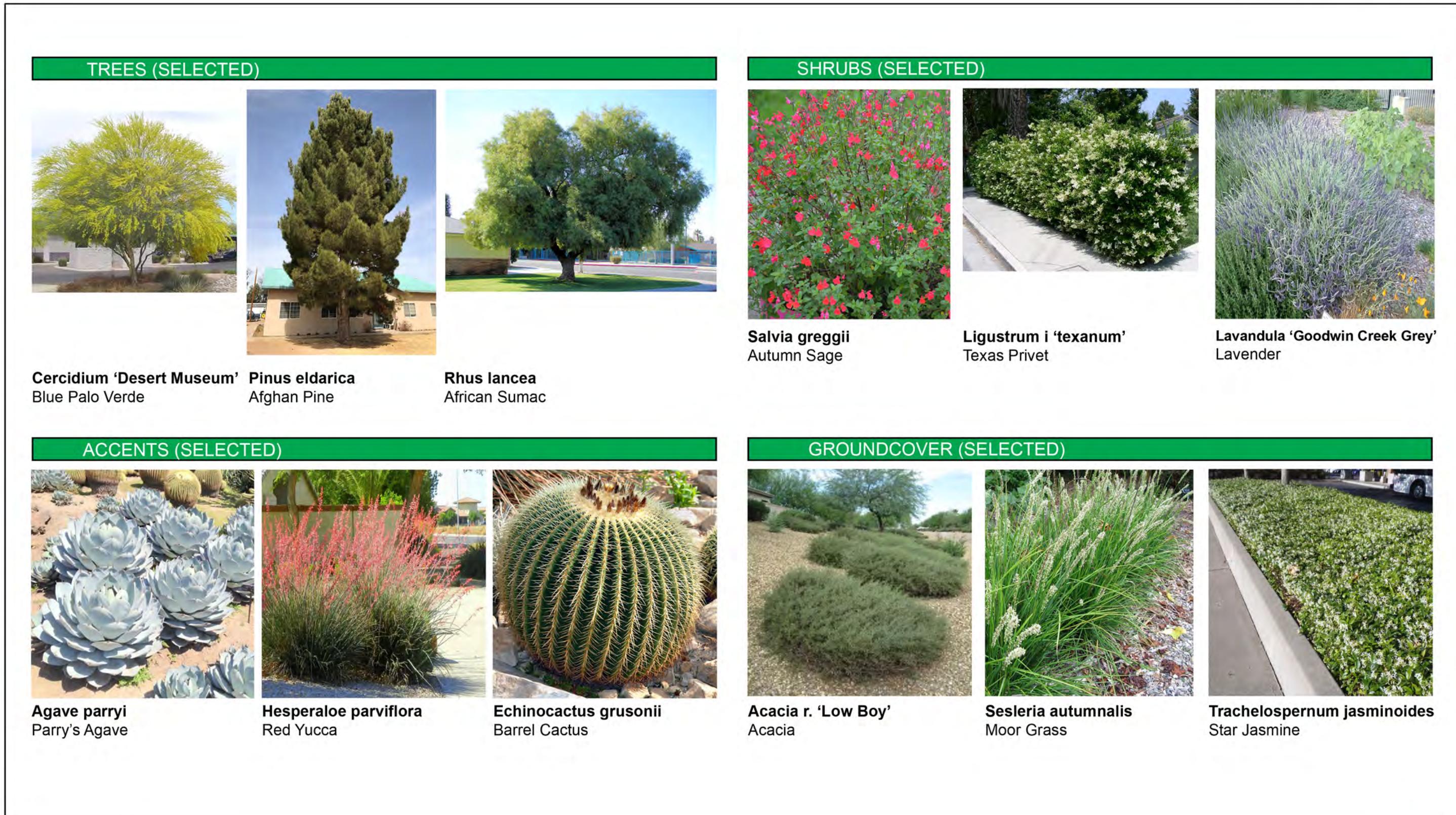
Source(s): ESRI, Nearmap (2022), Proactive Engineering Consultants (01-2023), Hunter Landscape (2021)

FIGURE 4-1



Source(s): Hunter Landscape (02-10-2021)

FIGURE 4-2



Source(s): Hunter Landscape (09-2021)

FIGURE 4-3

4.6.2 IRRIGATION

The following general irrigation concepts should be considered in the design and installation of irrigation systems within SUNSET CROSSROADS:

- (1) All landscaped areas should be equipped with a permanent, automatic, underground irrigation system. Drip systems and highly efficient rotators are encouraged in all areas needing irrigation. Irrigation systems must conform to all City of Banning requirements and State Model Water Efficient Landscape Ordinance AB1881.
- (2) Irrigation systems should be designed to apply water slowly, allowing plants to be deep soaked and to reduce run-off.
- (3) Connect the irrigation system to the recycled water conveyance system, if available.
- (4) "Pop-up" type sprinkler heads may be used adjacent to walks, drives, curbs (car overhangs), parking areas and public right-of-way but must be designed and maintained to prevent run-off and overspray.
- (5) The design of irrigation systems, particularly the location of controller boxes, valves, and other above-ground equipment (e.g., backflow prevention devices), should be incorporated into the overall landscaping design. Where aboveground equipment is provided, it should be screened or otherwise removed from public view to the extent possible.
- (6) The irrigation system should be programmed to operate between 8:00pm and 9:00am.
- (7) Non-functional turf, meaning turf that is not intended for active or passive recreation, is prohibited from use.

4.6.3 CONCEPTUAL PARK PLAN (PLANNING AREA 11)

A 5.0-acre public passive park is anticipated to be provided within Planning Area 11, along with 7.6 acres of open space and trails. As illustrated on Figure 4-4, *Conceptual Park Plan (PA 11)*, Planning Area 11 may provide recreational amenities such as, but not limited to, a tot lot playground, picnic tables, walking paths, trails, and restrooms. Open field areas are provided next to the playground and picnic tables, along with a looped walking path which winds through the recreational area surrounded by trees and landscaping, in accordance with Table 4-1, *Plant Palette*.

4.6.4 MONUMENTATION

Entry Monumentation has been created in order to identify arrival to SUNSET CROSSROADS, distinguish individual Planning Areas, and establish a sense of place consistent with the overall SUNSET CROSSROADS theme. The entry treatments have been designed to create a distinctive visual statement that emphasizes the SUNSET CROSSROADS' image as a high-quality, contemporary development.

The monumentation examples illustrated within these Design Guidelines represent consistent interpretations of SUNSET CROSSROADS' character and theme. Implemented monumentation need not match these examples exactly and may be designed with enough flexibility to respond to physical contexts along with the needs and desires of individual tenants, but all entry monuments should be generally consistent in theme and character. Consistency should be established through use of matching or complementary logos, type styles, and color schemes throughout the area being identified.

The conceptual location of entry monumentation is depicted on Figure 4-1, *Conceptual Master Landscape Plan*. Note that Figure 4-1 identifies the conceptual locations of entry monumentation and signage, the final locations of entry monuments and signs will be determined at the time internal streets, buildings, and driveway locations are identified as part of applications submitted within this Specific Plan).

❖ Conceptual Primary Monumentation

Primary Monumentation provided within SUNSET CROSSROADS creates an inviting atmosphere for employees and visitors by providing a form of wayfinding and sense of identity to SUNSET CROSSROADS. Primary Monumentation may be provided at the Sunset Avenue and Lincoln Street intersection. As shown on Figure 4-5, *Conceptual Primary Monumentation*, Primary Monumentation is made of fabricated aluminum, a faux corten color finish with the project logo, and a stone veneer base. The trees, shrubs, and groundcovers planted in the background and foreground of Primary Monumentation are in accordance with Table 4-1, *Plant Palette*. The final design and materials of this monument will be determined with applications submitted within this Specific Plan and/or as part of the Specific Plan Sign Program and Signage Design Guidelines.

❖ Conceptual General Commercial Monumentation

General Commercial Monumentation may be provided at individual building sites within Planning Area 1 to announce the arrival into the General Commercial area for employees and visitors. As shown on Figure 4-6, *Conceptual General Commercial Monumentation*, the General Commercial Monumentation consist of fabricated aluminum, a faux corten color finish with the project logo, a stone veneer base, and raised illuminated or non-illuminated letters of building tenants. The trees, shrubs, and groundcovers planted in the background and foreground of General Commercial Monumentation are in accordance with Table 4-1, *Plant Palette*. The final design and materials of this monument will be determined with applications submitted within this Specific Plan and/or as part of the Specific Plan Sign Program and Signage Design Guidelines.

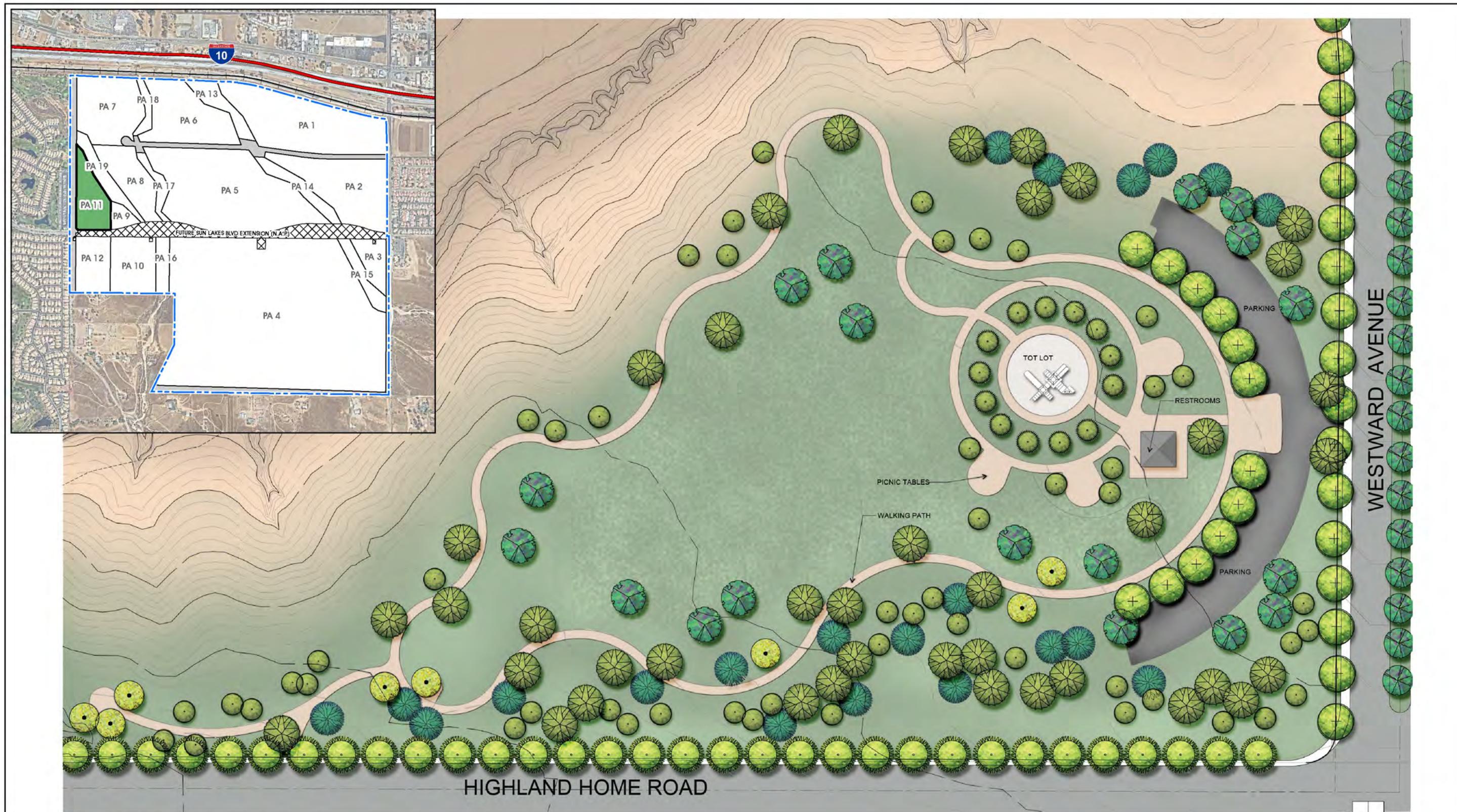
❖ Conceptual Industrial Monumentation

Industrial Monumentation may be provided at individual building sites within Planning Areas 2 through 10 to announce the arrival into the Industrial areas for employees and visitors. As shown on Figure 4-7, *Conceptual Industrial Monumentation*, Industrial Monumentation consist of fabricated aluminum, a faux corten color finish with the project logo, a stone veneer base, and raised illuminated or non-illuminated letters of building tenants. The trees, shrubs, and groundcovers planted in the background and foreground of the Industrial Monumentation are in accordance with Table 4-1, *Plant Palette*. The final design and materials of this monument will be determined with applications submitted within this Specific Plan and/or as part of the Specific Plan Sign Program and Signage Design Guidelines. Conceptual Tenant Monumentation

Tenant Monumentation may be provided at individual building sites within Planning Areas 1 through 10 to announce the arrival into the General Commercial and Industrial areas for employees and visitors. As shown on Figure 4-8, *Conceptual Tenant Monumentation*, Tenant Monumentation consists of fabricated aluminum, a faux corten color finish with the project logo, a stone veneer base, and raised illuminated or non-illuminated letters of building tenants. The trees, shrubs, and groundcovers planted in the background and foreground of the Tenant Monumentation are in accordance with Table 4-1, *Plant Palette*. The final locations of Tenant Monumentation will be determined at the time internal streets, buildings, and driveway locations are identified as part of applications submitted within this Specific Plan. The final design and materials of this monument will be determined with implementing project(s) and/or as part of the Specific Plan Sign Program and Signage Design Guidelines.

❖ Conceptual Freeway Oriented Freestanding Signs

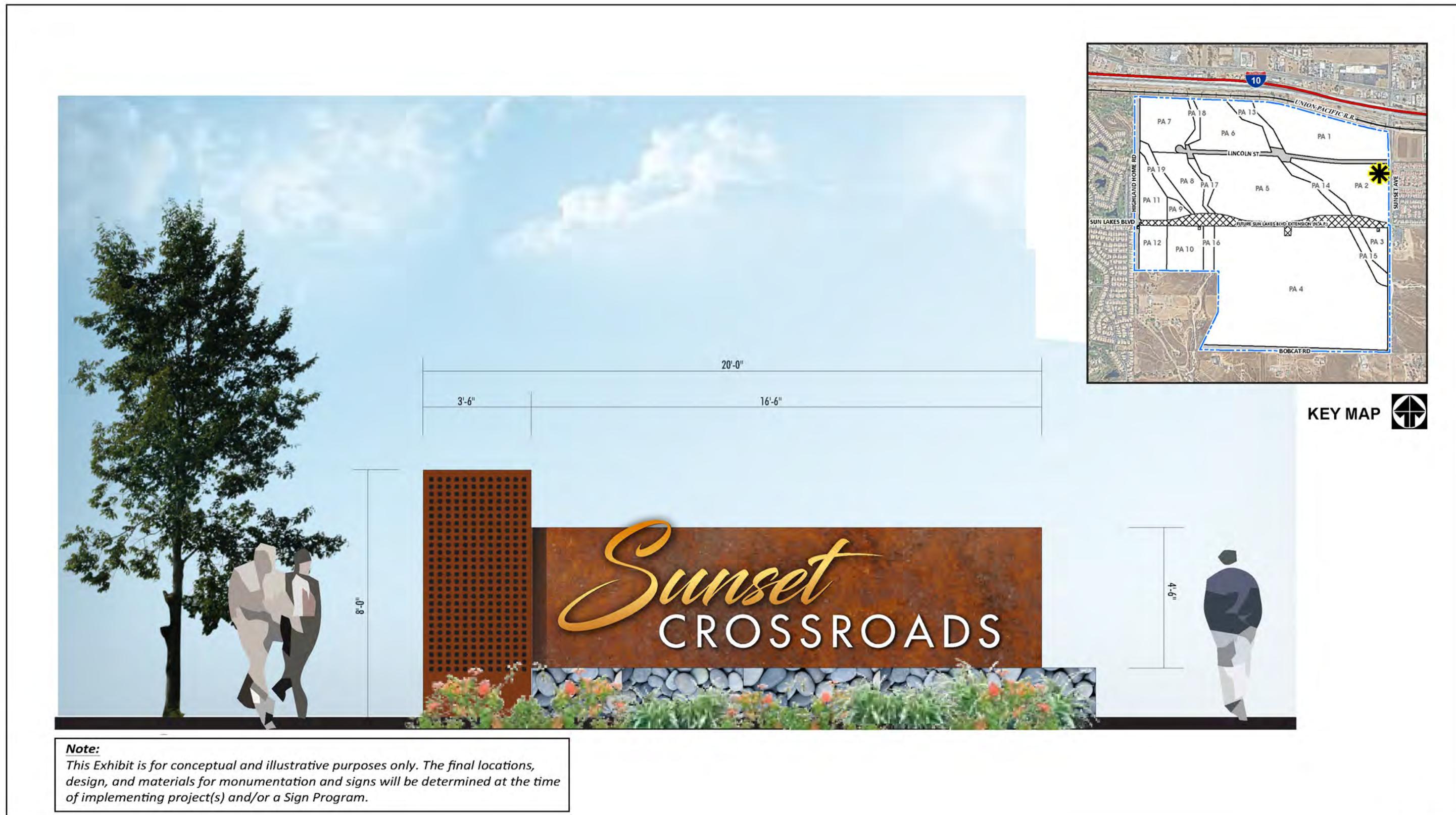
The final location, design, and materials of Freeway Oriented Freestanding Signs will be established by the SUNSET CROSSROADS Sign Program and will be selected to maximize exposure and enhance the visibility of SUNSET CROSSROADS from Interstate 10. Freeway Oriented Freestanding Signs include electronic message board pylon signs, freestanding monument signs, freestanding pylon signs, and freestanding tenant signs at a maximum height of 80 feet and maximum width of 30 feet. As illustrated on Figure 4-9, *Conceptual Freeway Oriented Freestanding Sign*, Freeway Oriented Freestanding Signs may consist of fabricated aluminum, a faux corten color finish with the project logo, a stone veneer base, and individually illuminated channel letters. The final design and materials of this sign will be determined with applications submitted within this Specific Plan and/or as part of the Specific Plan Sign Program and Signage Design Guidelines.



Source(s): Hunter Landscape (05-25-2021)

FIGURE 4-4





Source(s): ADS (03-15-2021)

FIGURE 4-5

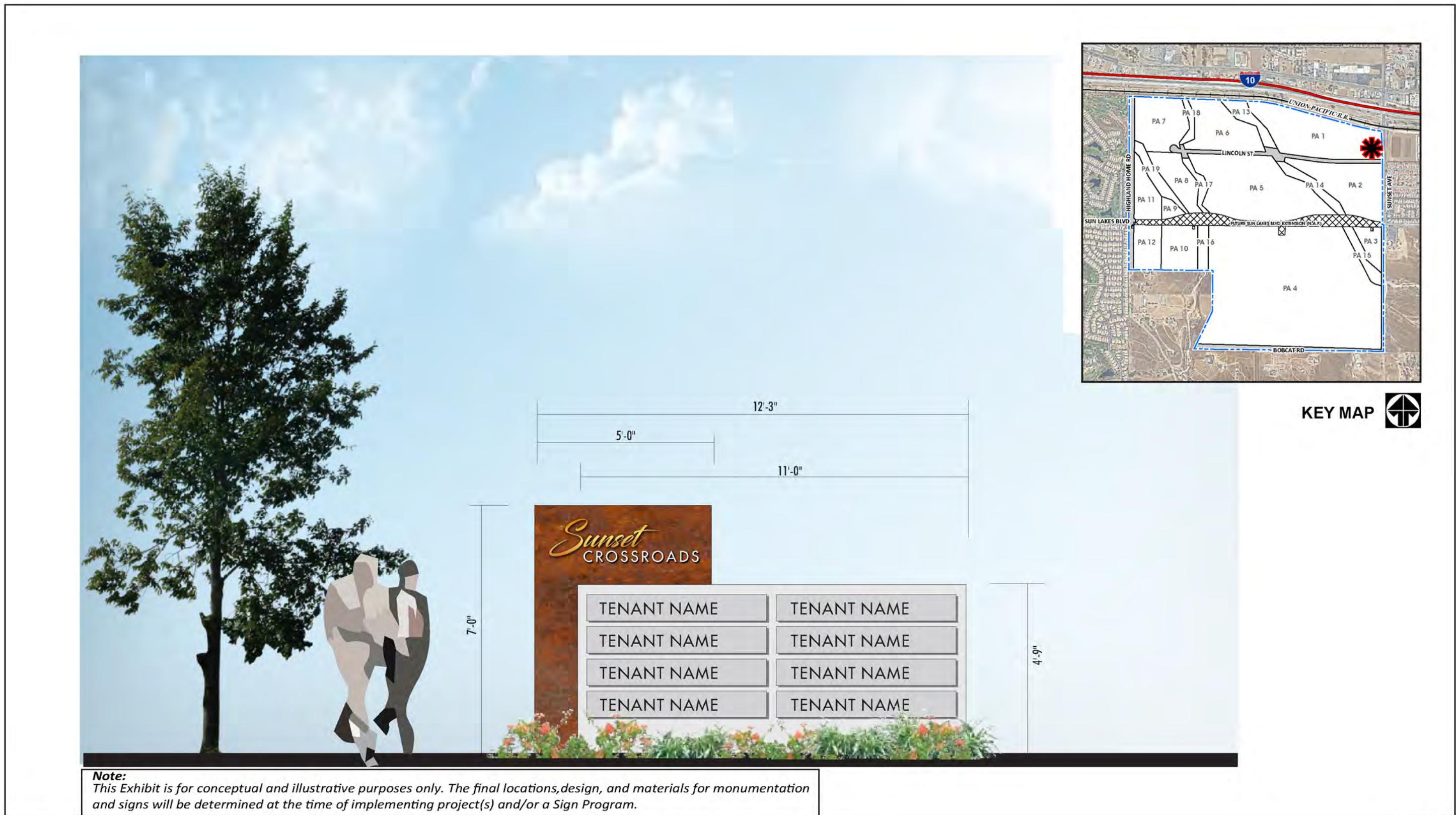
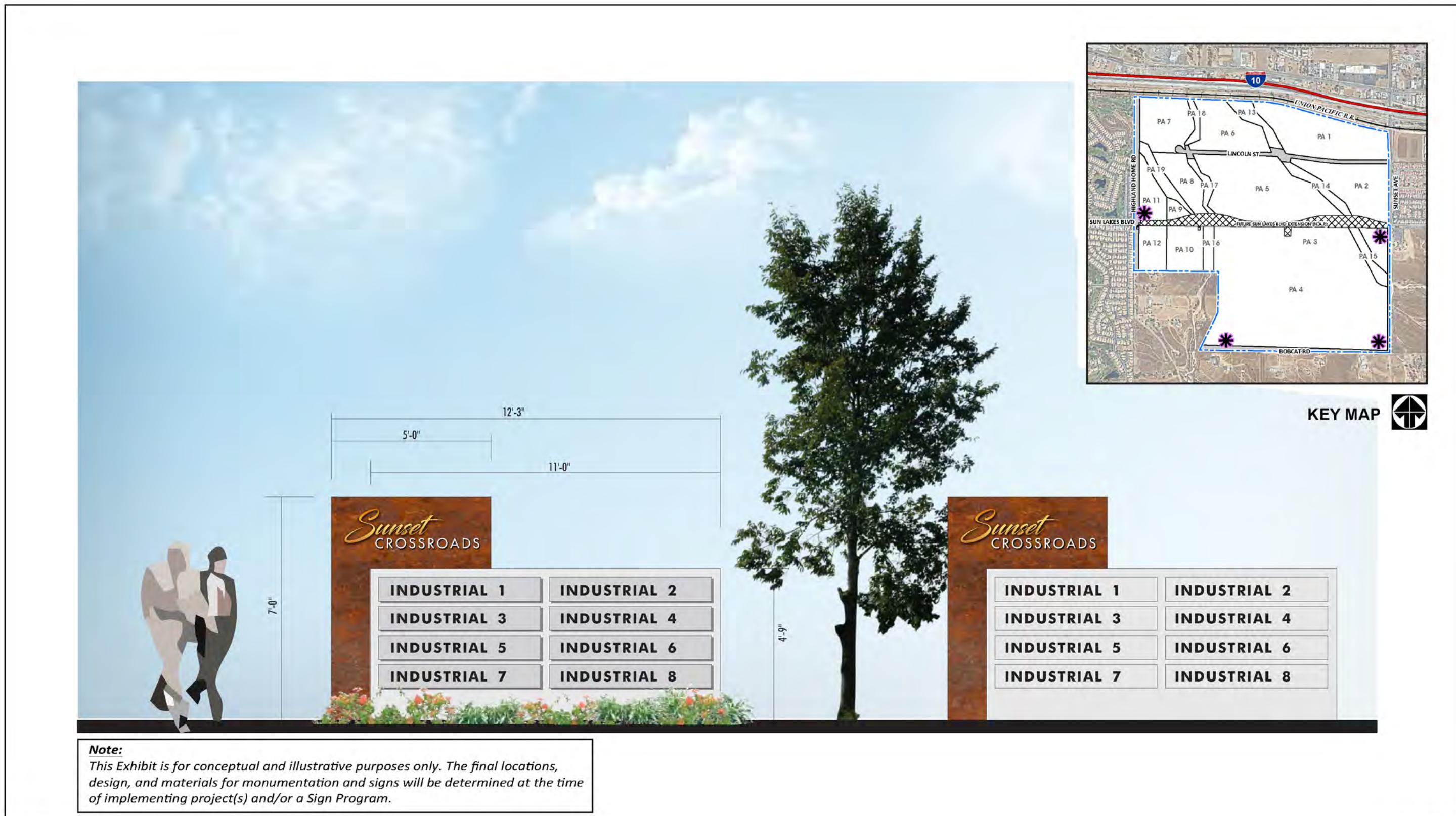
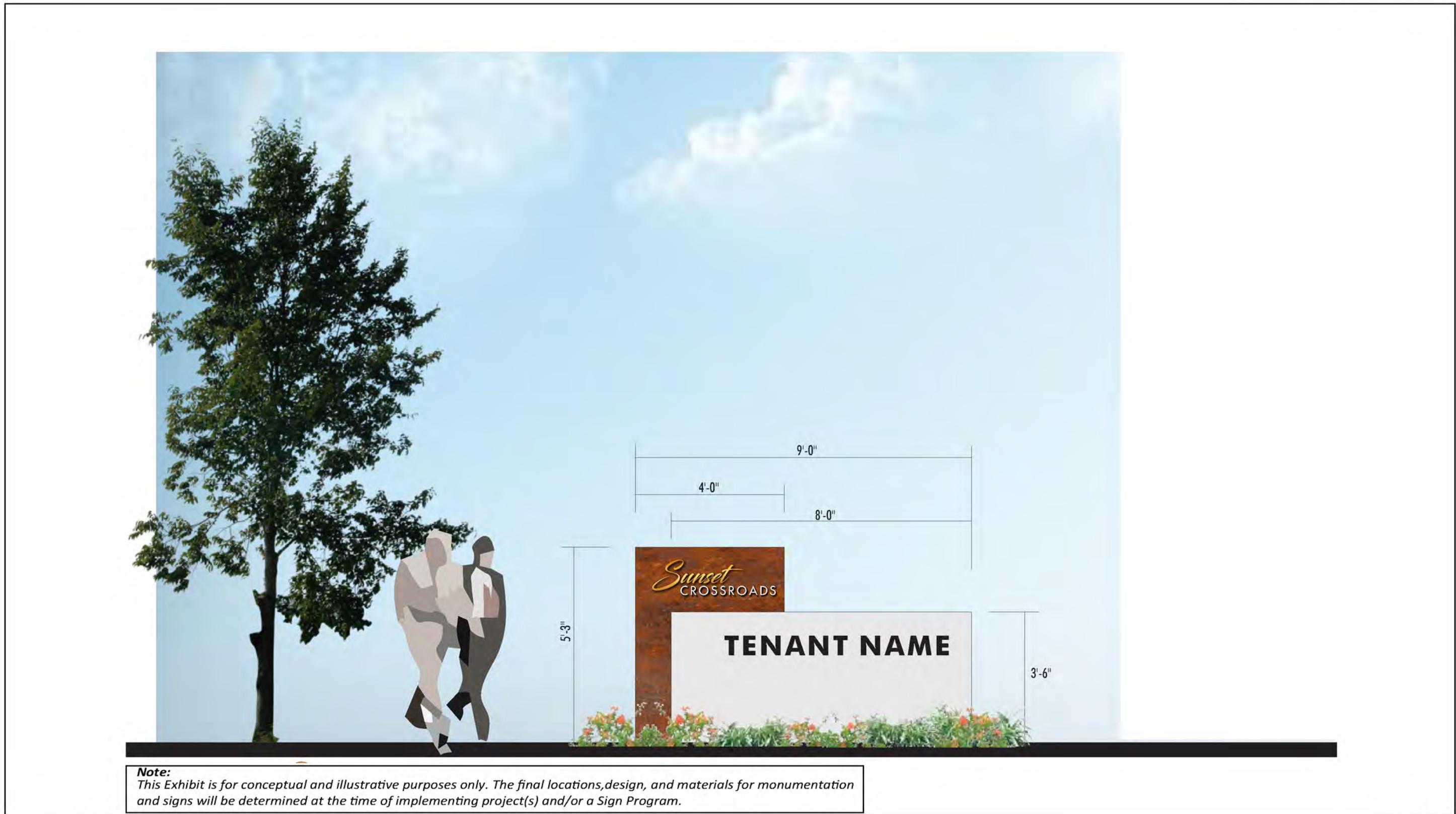


FIGURE 4-6



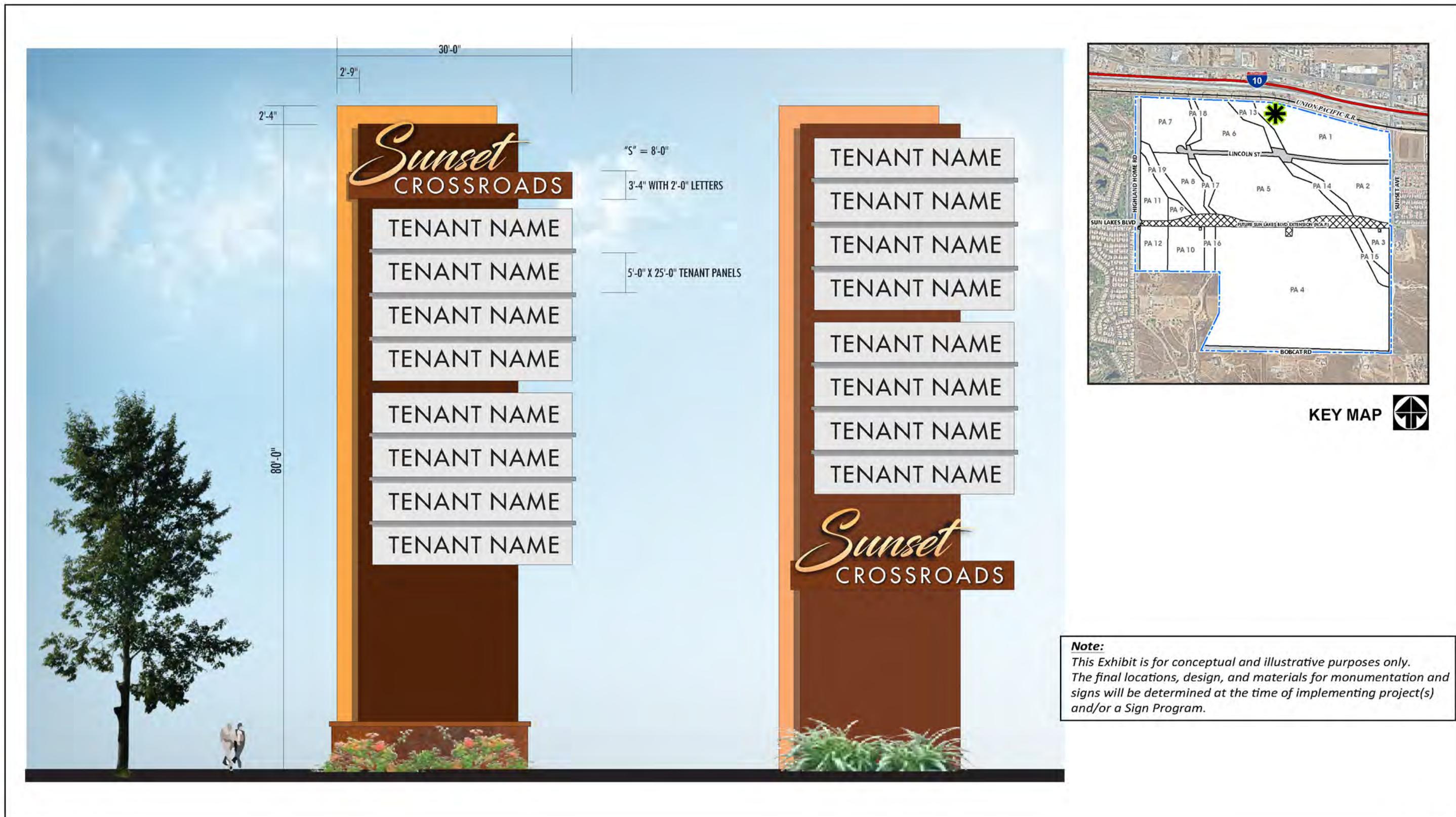
Source(s): ADS (03-15-2021)

FIGURE 4-7



Source(s): ADS (03-15-2021)

FIGURE 4-8



Source(s): ADS (03-15-2021)

FIGURE 4-9

4.6.5 STREETSCAPES

Streetscapes within SUNSET CROSSROADS are critical to creating a sense of place and maintaining the project's high-quality theme. In addition, streetscapes serve the functional purposes of softening and screening less desirable project elements from public view. Streetscapes throughout SUNSET CROSSROADS should be planted with a combination of street trees, shrubs, and large masses of groundcover. The landscaping Plant Palette for streetscapes links the roadways to the rest of SUNSET CROSSROADS by providing continuity throughout the entire Specific Plan using the Landscape Zones discussed in Section 4.6.1, *Plant Palette*.

Landscape treatments may include elements such as sidewalks, pedestrian paths, and parkway trees to enhance roadway appearances. Landscaping should consist of drought-tolerant plants, colorful shrubs, and street trees in accordance with Table 4-1, *Plant Palette*. Landscaping within the public right of way will be maintained by one or a combination of the following entities: the City of Banning, a Landscape, Lighting Maintenance District (LLMD), a Master Property Owners' Association (MPOA), or a Property Owners' Association (POA). In all instances, an appropriate line-of-sight for entering/exiting vehicles shall be maintained at street intersections within SUNSET CROSSROADS. The conceptual streetscape landscape treatments within SUNSET CROSSROADS are presented on the following pages.

❖ Conceptual Lincoln Street Streetscape (78' ROW)

As shown on Figure 4-10, Lincoln Street Streetscape is a 78-foot right-of-way. The right of way consists of 56 feet of paving with a 6-foot-wide landscaped parkway and 5-foot-wide parkway-adjacent sidewalk on one side of the street and a 6-foot-wide curb-adjacent sidewalk and 5-foot-wide landscaped parkway on the other side of the street. SUNSET CROSSROADS will be constructing the full width of this roadway. The trees, shrubs, and groundcovers planted in the landscape zones along Lincoln Street are in accordance with Table 4-1, *Plant Palette*.

❖ Conceptual Sunset Avenue Streetscape (I-10 to Lincoln Street) (110' ROW)

As shown on Figure 4-11, Sunset Avenue Streetscape from I-10 to Lincoln Street is a 110-foot right-of-way. The westerly portion of the right of way consists of a 9-foot median and 34 feet of paving including an 8-foot-wide Class II bike lane, with a 4-foot curb-adjacent landscaped parkway and an 8-foot-wide parkway-adjacent sidewalk. The easterly portion consists of a 9-foot median and 34 feet of paving including an 8-foot-wide Class II bike lane, with an 8-foot-wide curb-adjacent sidewalk and a 4-foot landscaped parkway. The trees, shrubs, and groundcovers planted in the landscape zones along Sunset Avenue are in accordance with Table 4-1, *Plant Palette*.

❖ Conceptual Sunset Avenue Streetscape (Lincoln Street to SLB Extension) (110' ROW)

As shown on Figure 4-11A, Sunset Avenue Streetscape from Lincoln Street to SLB Extension is a 110-foot right-of-way. Each side of the right of way consists of a 9-foot median and 34 feet of paving including an 8-foot-wide Class II bike lane, with a 4-foot landscaped parkway and an 8-foot-wide parkway-adjacent sidewalk. 17 feet of excess right of way to the east may be landscaped as a buffer. The trees, shrubs, and groundcovers planted in the landscape zones along Sunset Avenue are in accordance with Table 4-1, *Plant Palette*.

❖ Conceptual Sunset Avenue Streetscape (SLB Extension to Bobcat Road) (88' ROW)

As shown on Figure 4-11B, Sunset Avenue Streetscape from the SLB Extension to Bobcat Road is an 88-foot right-of-way. Each side of the right of way consists of 32 feet of paving, with a 4-foot curb-adjacent landscaped parkway and an 8-foot parkway-adjacent sidewalk. The trees, shrubs, and groundcovers planted in the landscape zones along Sunset Avenue are in accordance with Table 4-1, *Plant Palette*.

❖ Conceptual Bobcat Road Streetscape (78' ROW)

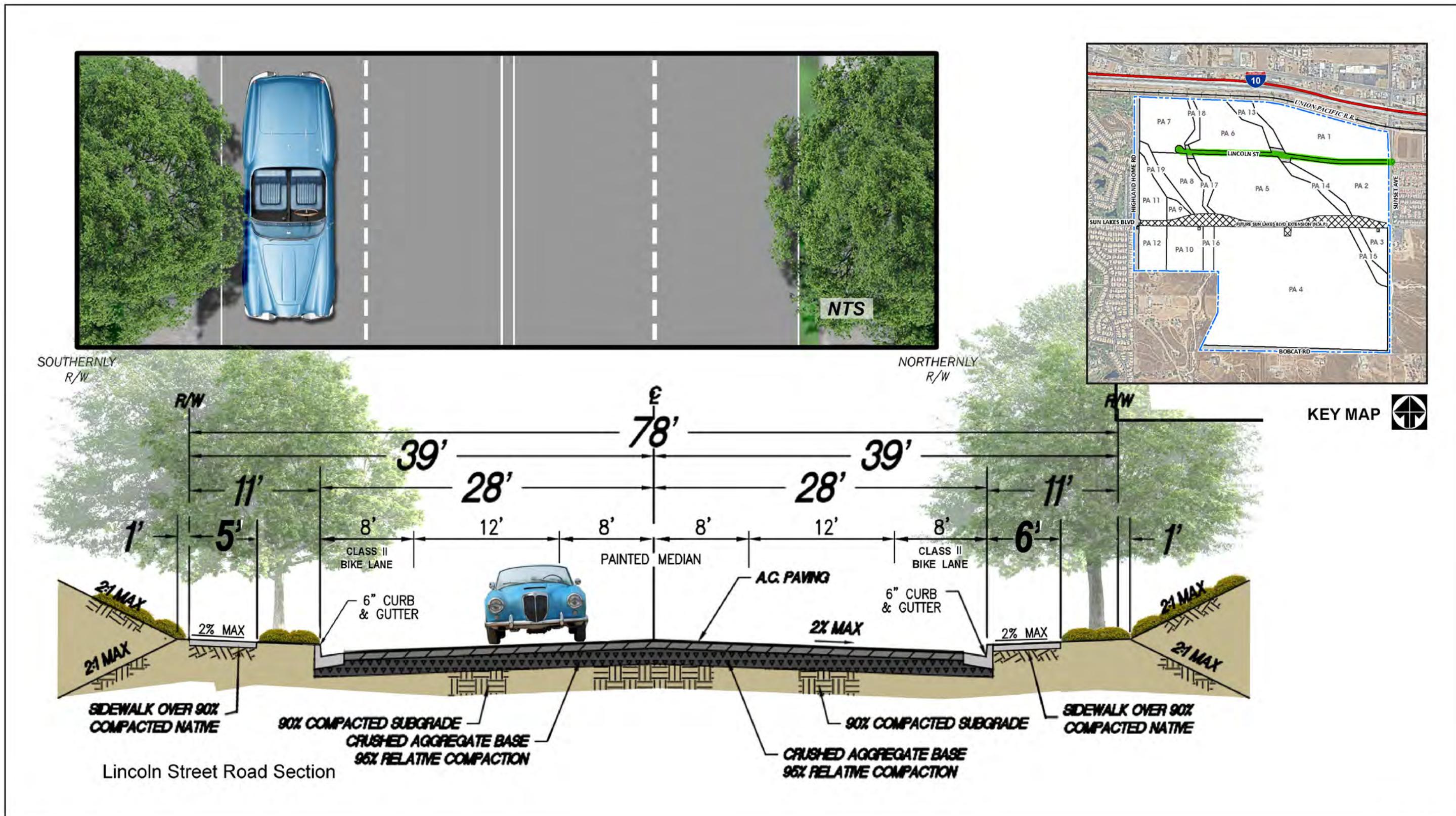
As shown on Figure 4-12, Bobcat Road Streetscape is a 78-foot right-of-way. The northerly portion of the right of way consists of 28 feet of paving with a 5-foot-wide landscaped parkway and 6-foot-wide curb-adjacent sidewalk. SUNSET CROSSROADS will be constructing only the north half-width plus 10' of this roadway. The trees, shrubs, and groundcovers planted in the landscape zones along Bobcat Road are in accordance with Table 4-1, *Plant Palette*.

❖ Conceptual Highland Home Road Streetscape (north of SLB Extension) (66' ROW)

As shown on Figure 4-13, Highland Home Road Streetscape (north of SLB Extension) is a 66-foot right-of-way within a 110-foot dedication. The western portion of the right of way consists of a 5-foot-wide curb-adjacent sidewalk, a 6-foot-wide landscaped parkway, and 44 feet of paving, including a Class III bikeway adjacent to the curb. SUNSET CROSSROADS will construct the half-width plus 10' of paving along the western boundary, which will terminate at the northern end with a cul-de-sac. The trees, shrubs, and groundcovers planted in the landscape zones along Highland Home Road are in accordance with Table 4-1, *Plant Palette*.

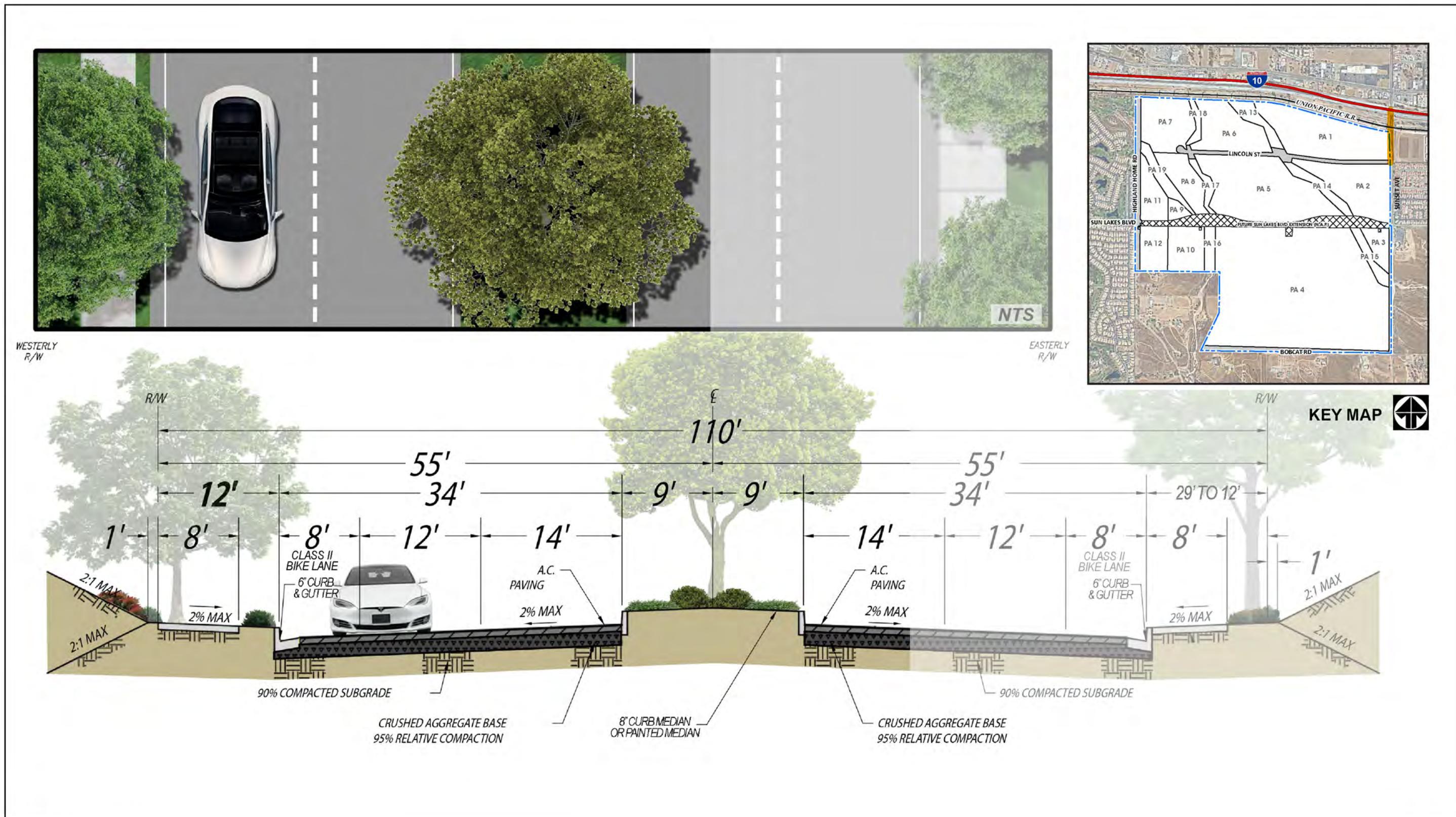
❖ Conceptual Highland Home Road Streetscape (south of SLB Extension) (66' ROW)

As shown on Figure 4-13A, Highland Home Road Streetscape (south of SLB Extension) is a 66-foot right-of-way within a 110-foot dedication. The eastern portion of the right of way consists of 22 feet of paving, including a Class III bikeway, a 6-foot-wide curb-adjacent sidewalk, and a 5-foot-wide landscaped parkway. SUNSET CROSSROADS will complete the existing improvements on the western side of the right-of-way. The trees, shrubs, and groundcovers planted in the landscape zones along Highland Home Road are in accordance with Table 4-1, *Plant Palette*.



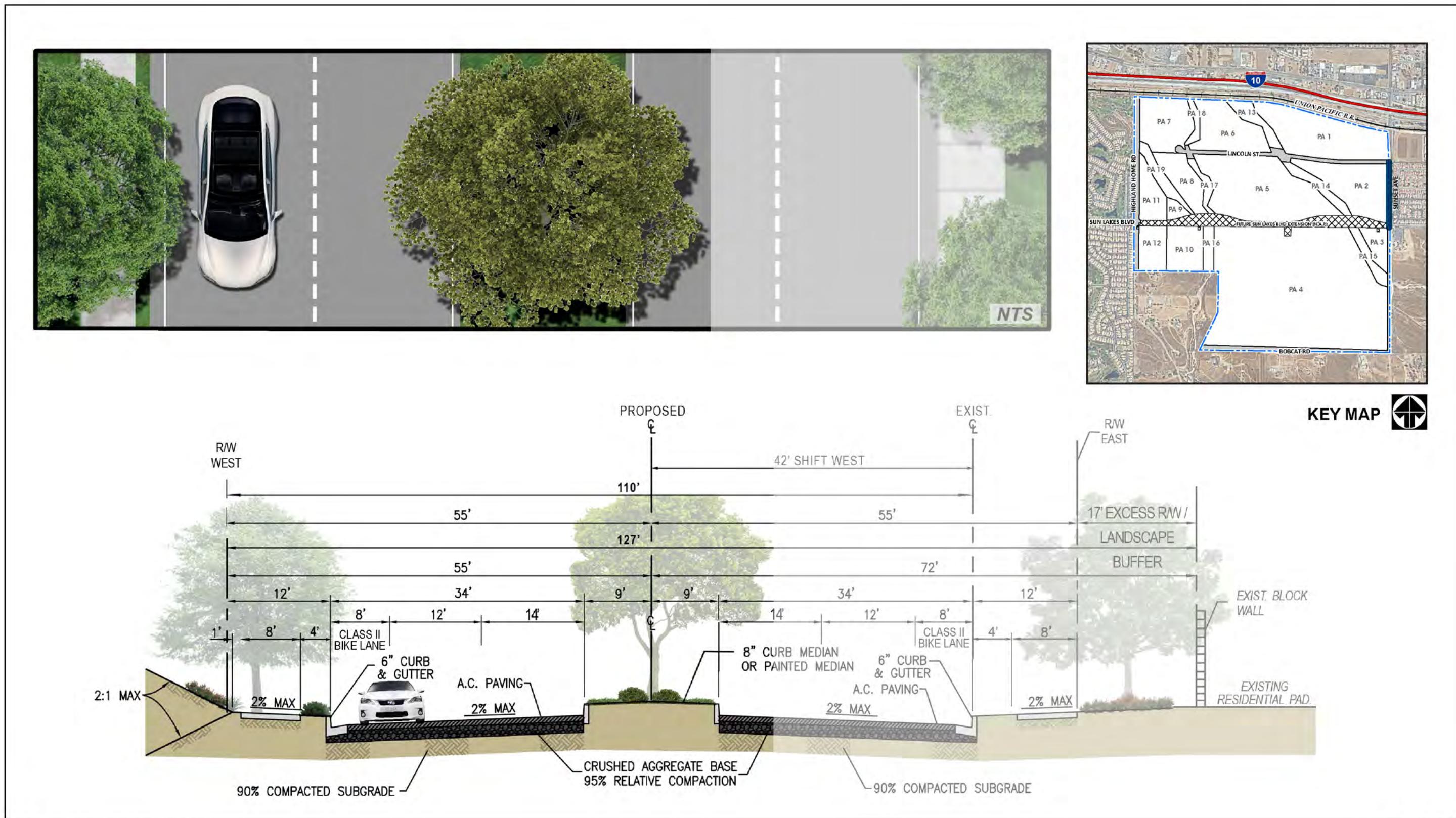
Source(s): Hunter Landscape (12-02-2022)

FIGURE 4-10



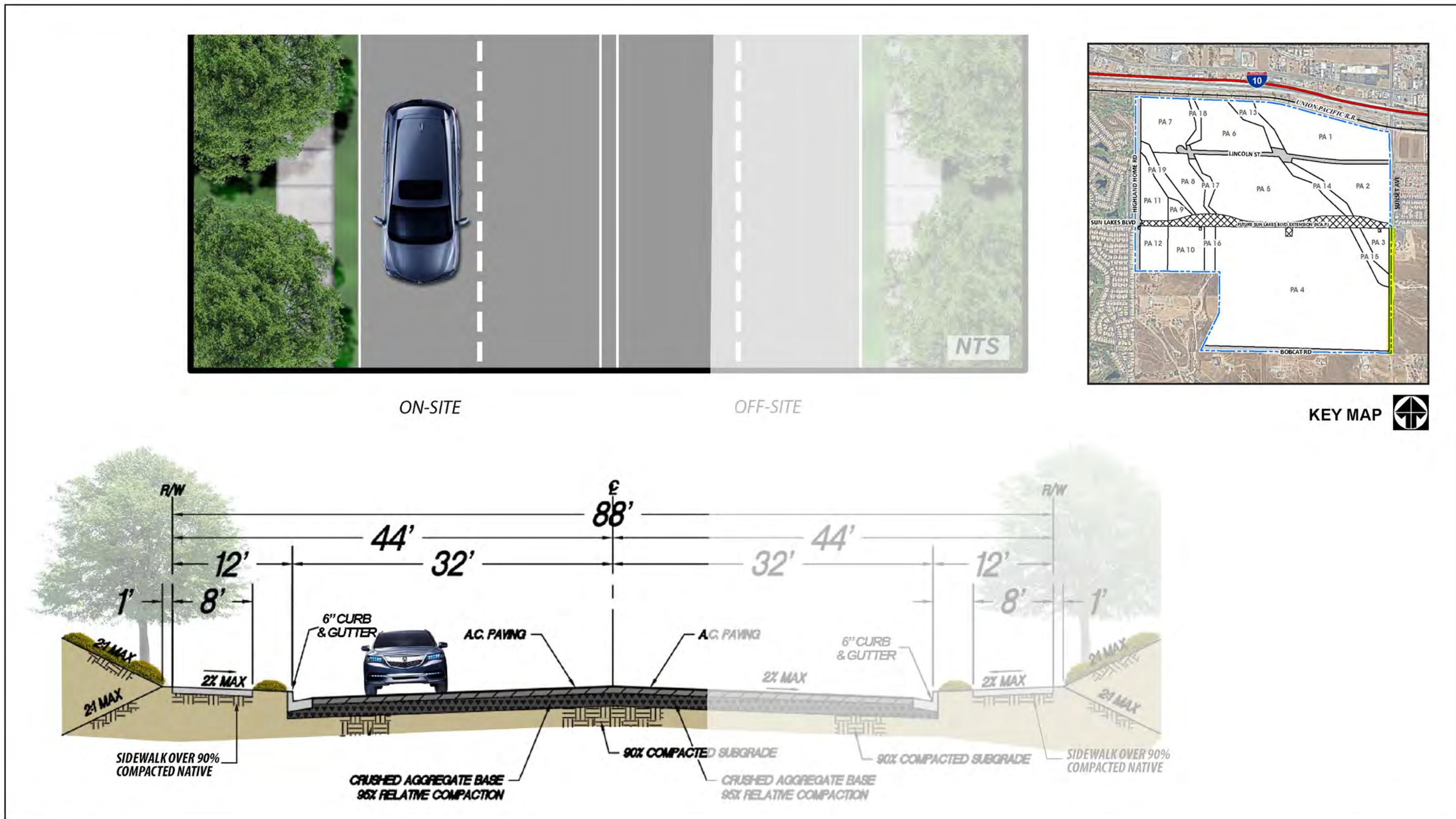
Source(s): Proactive Engineering Consultants (10-12-2023)

FIGURE 4-11



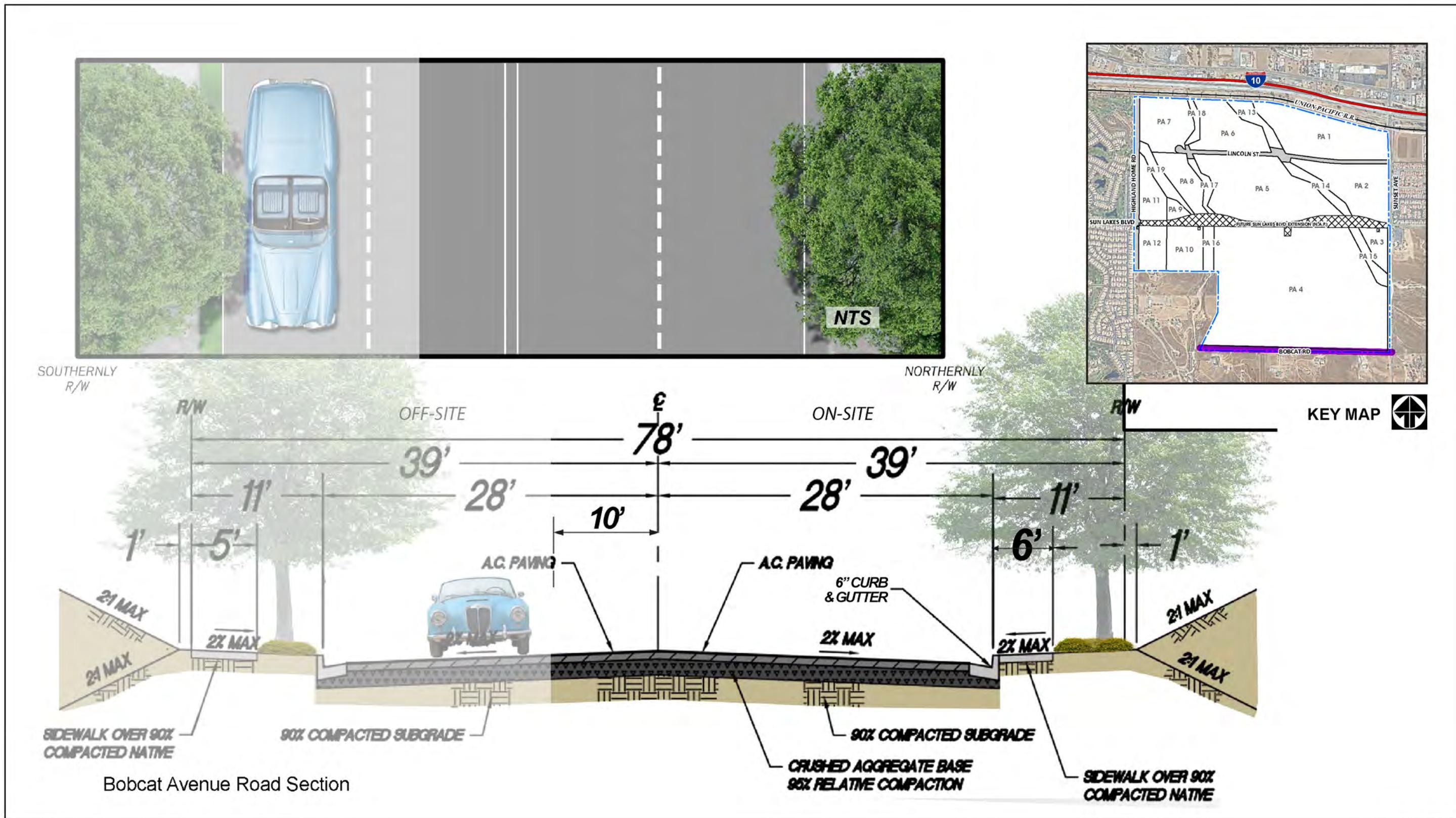
Source(s): Proactive Engineering Consultants (03-06-2024)

FIGURE 4-11A



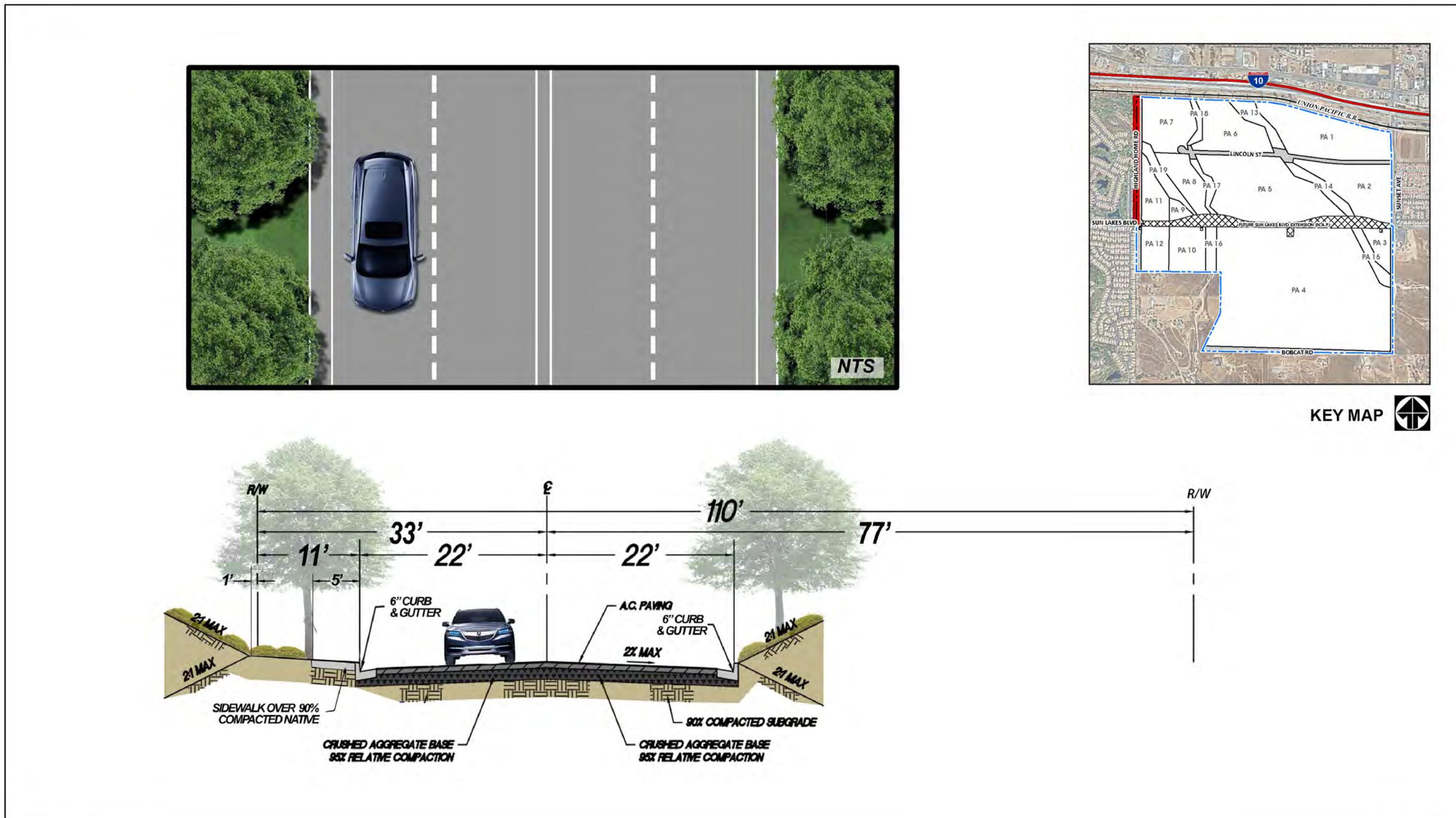
Source(s): Hunter Landscape (02-10-2021)

FIGURE 4-11B



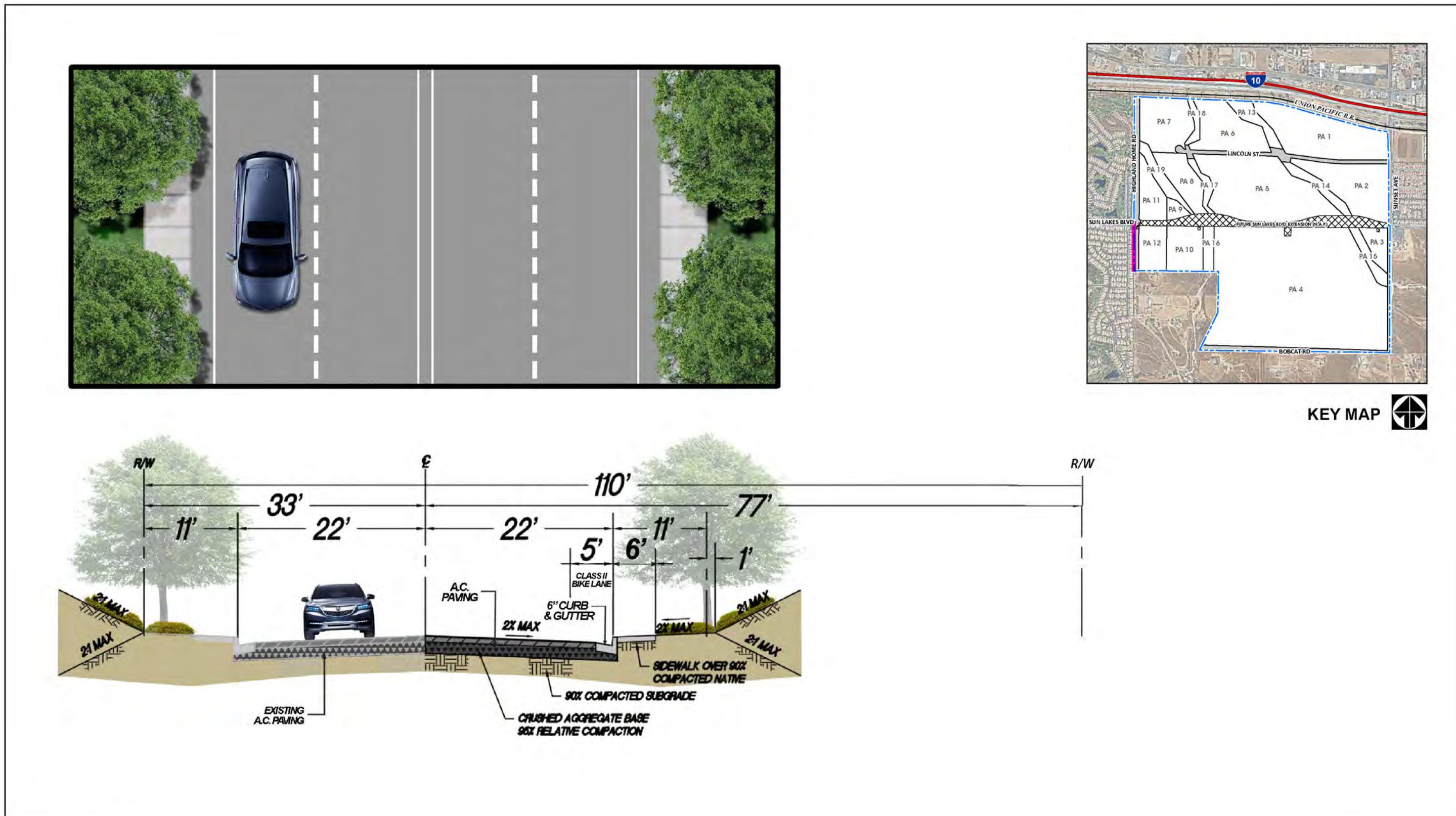
Source(s): Hunter Landscape (01-27-2023)

FIGURE 4-12



Source(s): Hunter Landscape (01-27-2023)

FIGURE 4-13



Source(s): Hunter Landscape (01-27-2023)

FIGURE 4-13A

4.6.6 WALL AND FENCES

Along building site perimeters and interior to building sites, walls and fences may be provided for screening, buffering, and security purposes within SUNSET CROSSROADS. Tubular steel fences with pilaster and CMU screen may be provided around the perimeters of individual building sites. Walls and fences may be provided around loading and dock areas, trailer parking areas, and parking lots and should be adequate to screen on-site uses from abutting public views and public roads. Landscaping may also be used to provide screening of SUNSET CROSSROADS' land uses from off-site areas. Figure 4-14, *Conceptual Wall and Fence Details*, illustrate the walls and fences which may be provided within the Specific Plan area. The final locations, materials, and details of these walls and fences will be determined when buildings are designed and oriented within a Planning Area as part of any applications submitted within this Specific Plan.

❖ Tubular Steel Fence

Tubular Steel Fences may be provided around the perimeters of General Commercial and Industrial land uses from roadways, water quality basins, loading and dock areas, truck yards, parking lots, within individual building sites, and/or as an alternative to CMU screen walls, when screening is not required. Tubular Steel Fences have a maximum height of 8' and include black tubular steel rods with pilasters and decorative concrete caps to be provided and spaced approximately 25' on-center. The final locations, materials, and details will be determined when buildings are designed and oriented within a Planning Area as part of any applications submitted within this Specific Plan.

❖ CMU Screen Wall

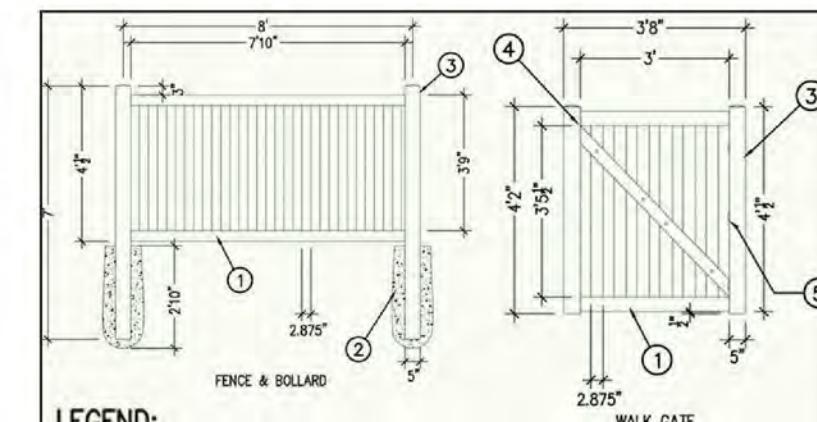
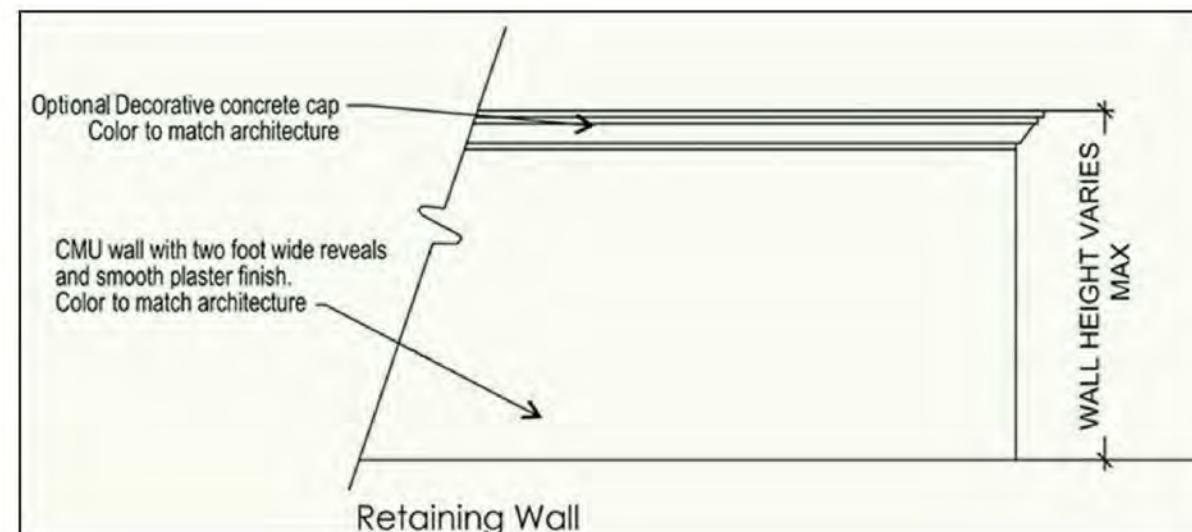
CMU Screen Walls may be provided within SUNSET CROSSROADS around the perimeters of Industrial land uses from roadways and Open Space-Resource Planning Areas. CMU Screen Walls have a minimum height of 6' and include warm architectural colors, concrete masonry unit walls and decorative concrete caps. CMU Screen Walls may be provided as an alternative to tubular steel fences, where screening may be required. The final locations, materials, and details will be determined when buildings are designed and oriented within a Planning Area as part of any applications submitted within this Specific Plan.

❖ Vinyl Fence and Gate

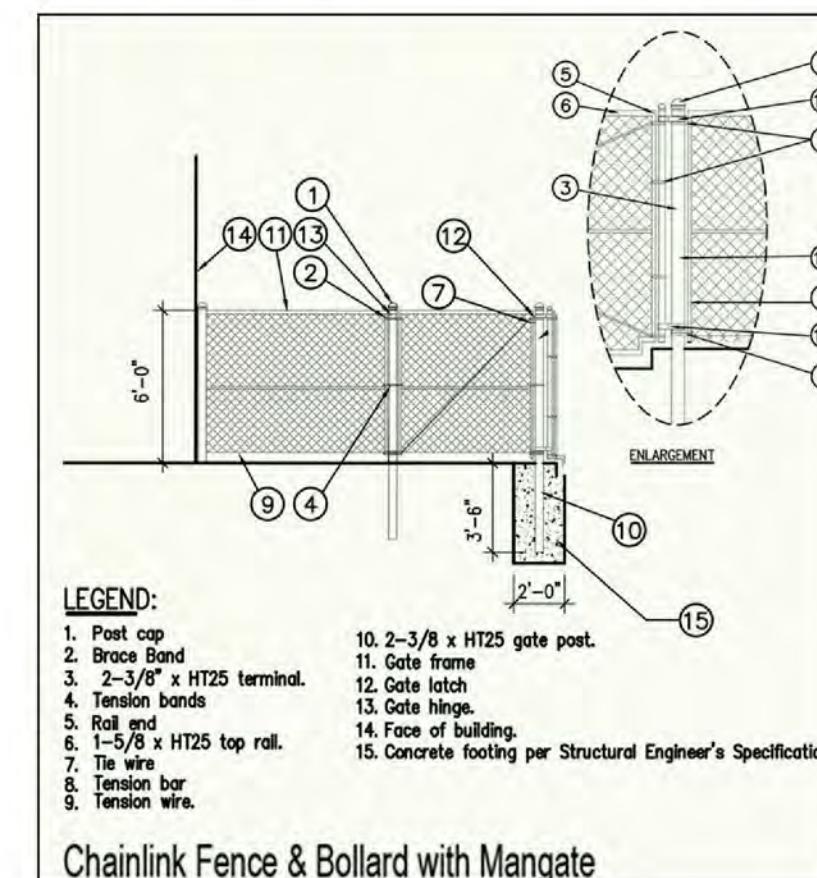
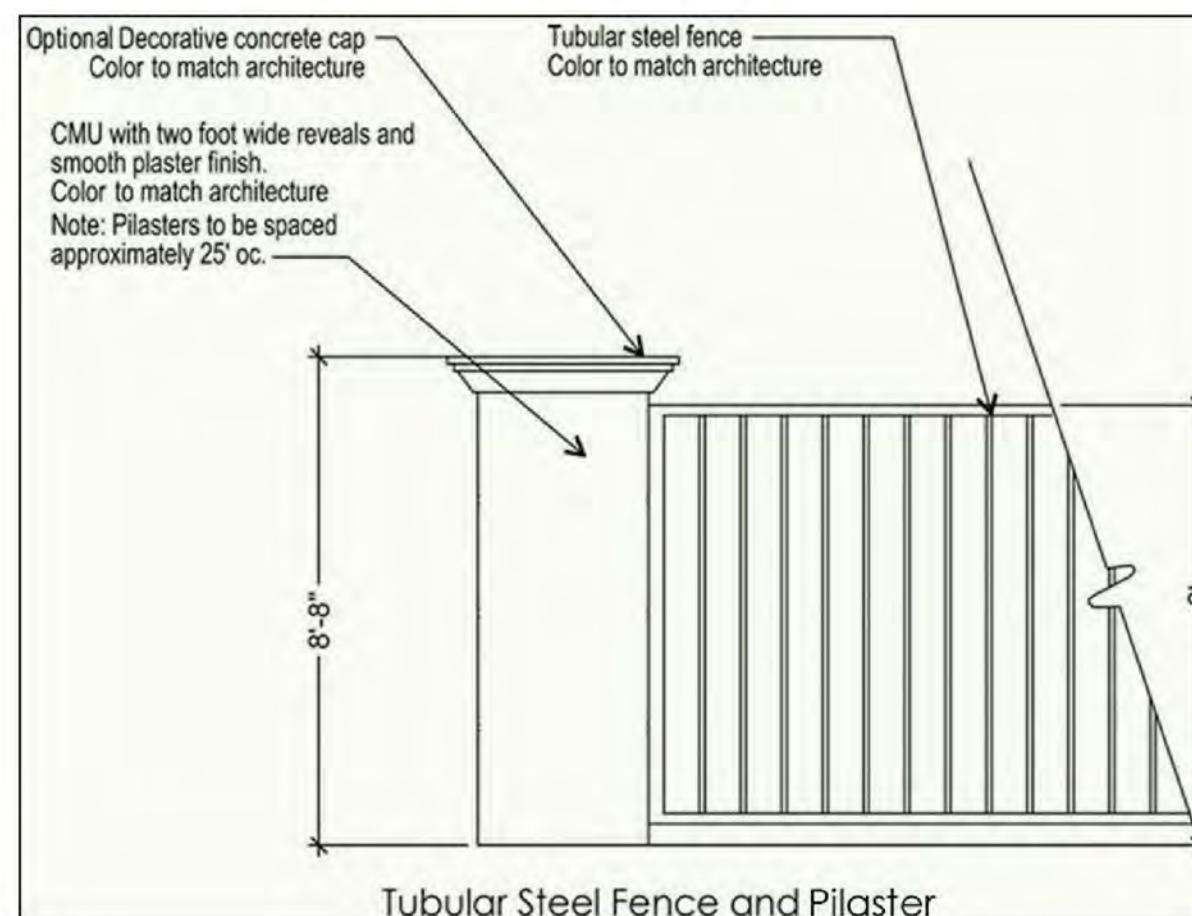
Vinyl Fences with gates may be provided within SUNSET CROSSROADS. Vinyl fences with gates have a minimum height of 4' 2" and consist of aluminum insert reinforcement, concrete footing, fence posts, gates with latches and hinges. The final locations, materials, and details will be determined when buildings are designed and oriented within a Planning Area as part of any applications submitted within this Specific Plan.

❖ Chainlink Fence and Bollard with Mangate

Chainlink Fences with bollards and mangates may be provided within SUNSET CROSSROADS. Chainlink Fences have a minimum height of 6' and consist of chain link tension wires, tension bars, rail ends, and post caps. The final locations, materials, and details will be determined when buildings are designed and oriented within a Planning Area as part of any applications submitted within this Specific Plan.



Vinyl Fence & Gate Detail



Chainlink Fence & Bollard with Mangate

Source(s): Hunter Landscape (05-25-2021)

FIGURE 4-14

4.6.7 LANDSCAPE INTERFACES

SUNSET CROSSROADS contains seven (7) distinct Landscape Interfaces, or edge treatments, which buffer at the boundaries of adjacent off-site and dissimilar on-site land uses. The general location of each of these Landscape Interfaces is depicted on Figure 4-1, *Conceptual Master Landscape Plan*. Each of these landscape interfaces are discussed in detail below. The interfaces depicted in this section may be modified by the requirements for fuel modification or brush clearing associated with future applications submitted within this Specific Plan. The types of landscape material and width may be modified if needed to address final building orientations or fire safety. Walls and fences may be provided within these interface locations during the design and implementation of the Specific Plan.

1. Conceptual Interface #1 – Commercial (Planning Area 1) to Open Space & Industrial (Planning Area 6)

Interface #1, as illustrated on Figure 4-15, identifies the interface condition where General Commercial land uses within Planning Area 1 are adjacent to existing natural drainage features within Planning Area 13 to the west and Industrial land uses within Planning Area 6. In this condition, General Commercial land uses are buffered from the existing natural drainage features within Planning Area 13 by a landscape buffer within Planning Area 1. Industrial land uses in Planning Area 6 are buffered from the existing natural drainage features within Planning Area 13 by a basin and a 15-foot-wide sewer easement. The trees, shrubs, and groundcovers within this interface are planted in accordance with Table 4-1, *Plant Palette*.

2. Conceptual Interface #2 – Industrial (Planning Area 2) to Off-Site Residential (East)

Interface #2, as illustrated on Figure 4-16, identifies the interface condition where Industrial land uses within Planning Area 2 are adjacent to the off-site residential uses located to the east. In this condition, Industrial land uses are buffered from the off-site uses by a landscape buffer and the 110-foot-wide right-of-way of Sunset Avenue and 17 feet of excess right of way. The trees, shrubs, and groundcovers within this interface are planted in accordance with Table 4-1, *Plant Palette*.

3. Conceptual Interface #3 – Industrial (Planning Area 3) to Off-Site School Site (East)

Interface #3, as illustrated on Figure 4-17, identifies the interface condition where Industrial land uses within Planning Area 3 are adjacent to the off-site Mount San Jacinto School Site to the east. In this condition, Industrial land uses are buffered by a landscape buffer along the perimeter of Planning Area 3 and Sunset Avenue. The eastern side of Sunset Avenue contains an existing, built-out 5-foot landscaped parkway and 5-foot-wide non-curb adjacent sidewalk, fronting the MSJC school site. The trees, shrubs, and groundcovers within this interface are planted in accordance with Table 4-1, *Plant Palette*.

4. Conceptual Interface #4 – Industrial (Planning Area 4) to Off-Site Residential (South)

Interface #4, as illustrated on Figure 4-18, identifies the interface condition where Industrial land uses within Planning Area 4 are adjacent to the off-site residential land uses to the south. In this condition, Industrial land uses are buffered from the off-site residential land uses by an infiltration basin, and the 78-foot-wide right-of-way of Bobcat Road. The trees, shrubs, and groundcovers within this interface are planted in accordance with Table 4-1, *Plant Palette*.

5. Conceptual Interface #5 – Industrial (Planning Area 4) to Off-Site Open Space

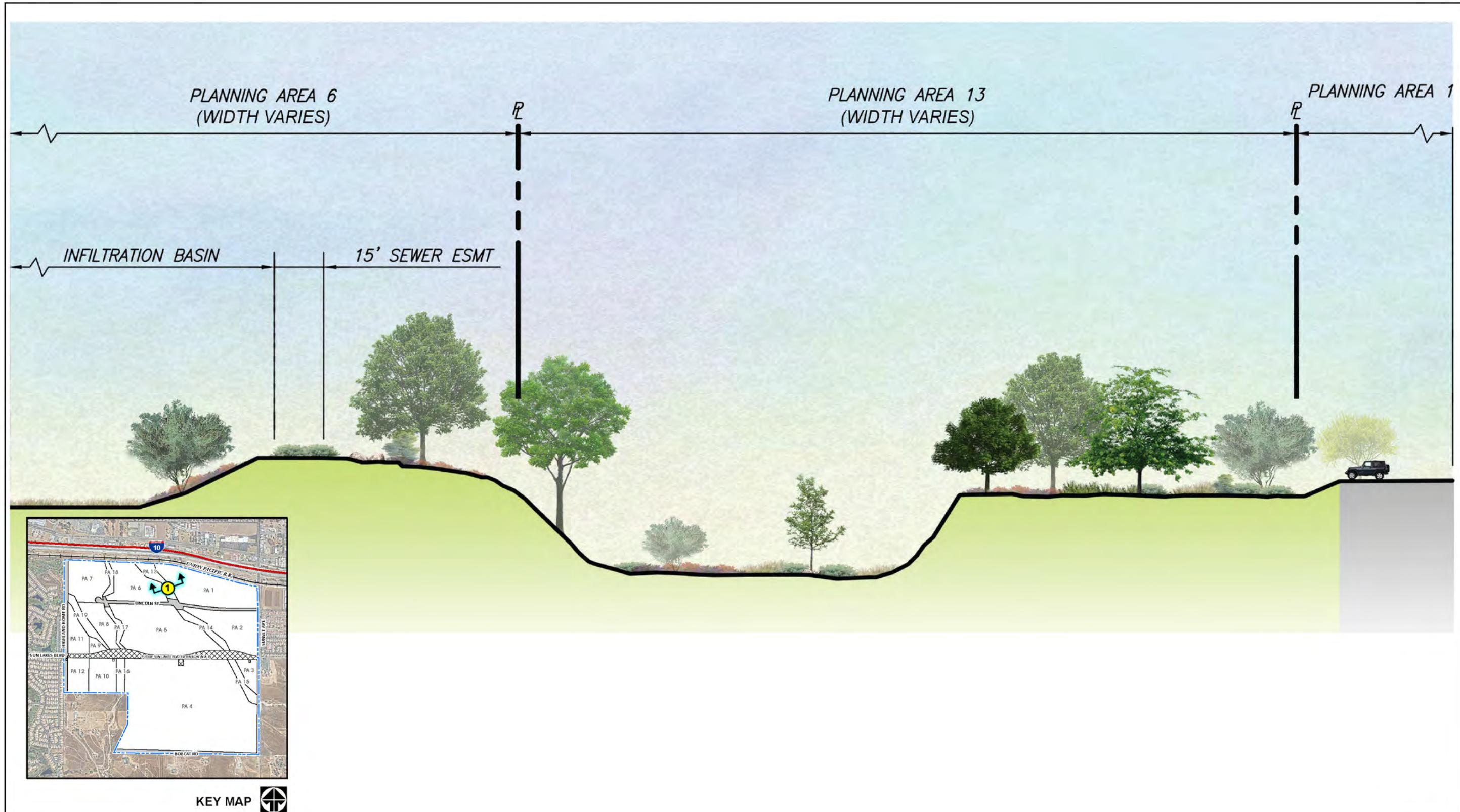
Interface #5, as illustrated on Figure 4-19, identifies the interface condition where Industrial land uses within Planning Area 4 are adjacent to the off-site open space areas to the west. In this condition, Industrial land uses are buffered from the off-site open space areas by a parking area, 40-foot-wide private drive aisle, and a landscape buffer. The trees, shrubs, and groundcovers within this interface are planted in accordance with Table 4-1, *Plant Palette*.

6. Conceptual Interface #6 – Industrial (Planning Area 12) to Off-Site Residential (West)

Interface #6, as illustrated on Figure 4-20, identifies the interface condition where the open space areas within Planning Area 12 are adjacent to the off-site residential land uses to the west. In this condition, existing natural drainage features within Planning Area 12 and the 110-foot-wide right-of-way of Highland Home Road act as a buffer for the residential land uses to the west. The trees, shrubs, and groundcovers within this interface are planted in accordance with Table 4-1, *Plant Palette*.

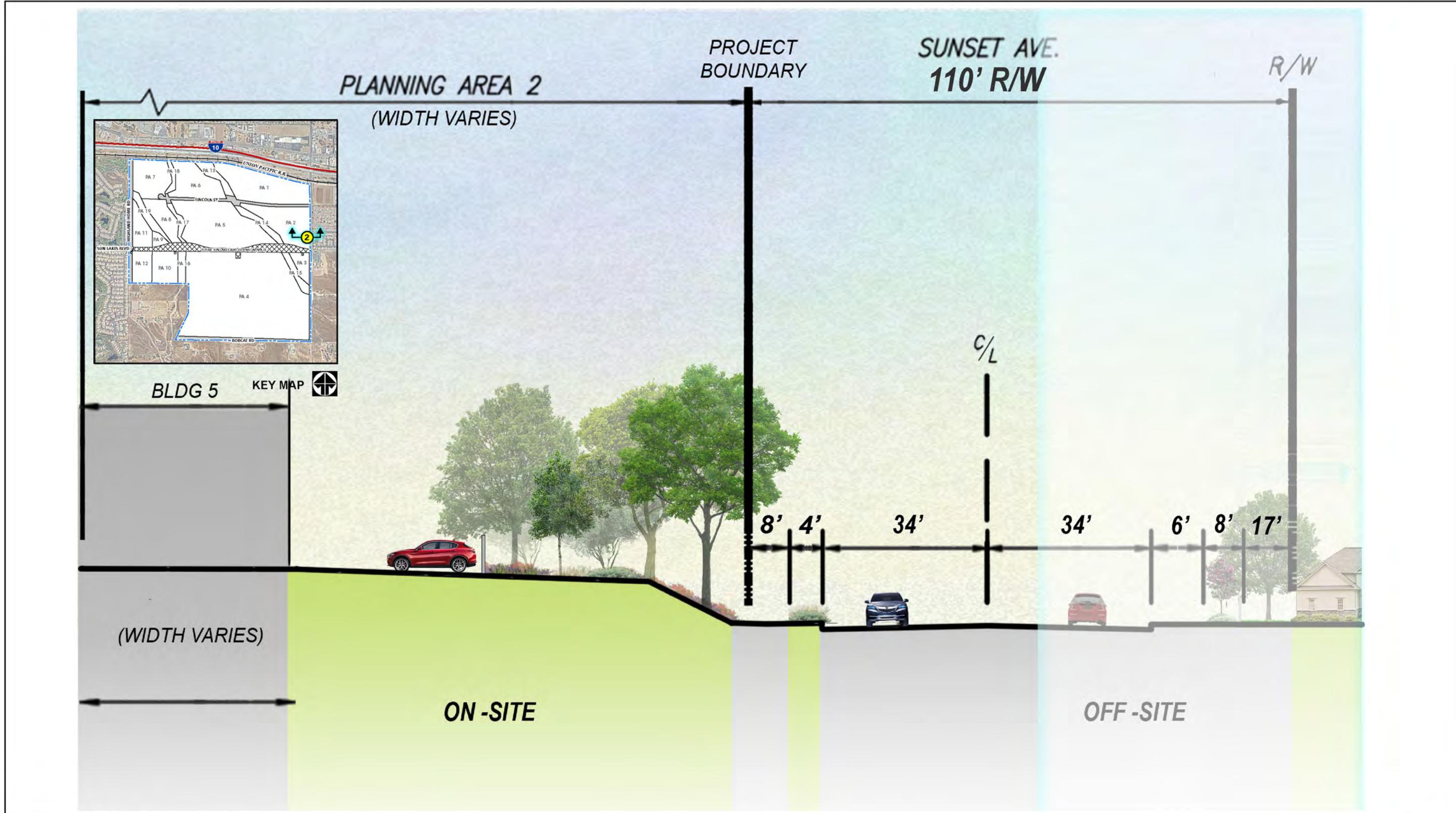
7. Conceptual Interface #7 – Industrial (Planning Area 7) to Off-Site Residential (West)

Interface #7, as illustrated on Figure 4-21, identifies the interface condition where Industrial land uses within Planning Area 7 are adjacent to the off-site residential land uses to the west. In this condition, Industrial land uses are buffered from the off-site residential land uses by a parking area, a landscape buffer up to 250-foot wide in some locations, and the 110-foot right-of-way of Highland Home Road. The trees, shrubs, and groundcovers within this interface are planted in accordance with Table 4-1, *Plant Palette*.



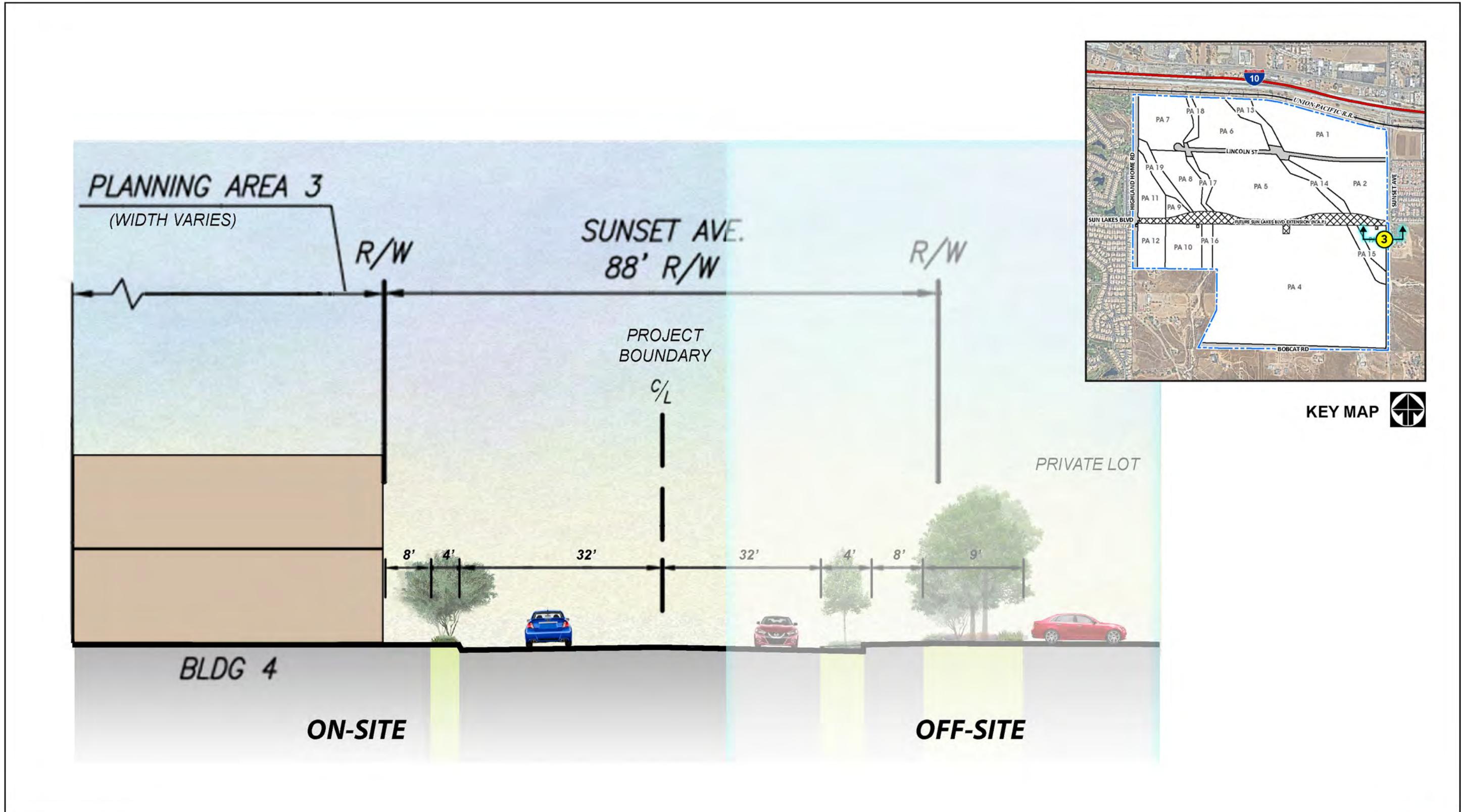
Source(s): Hunter Landscape (04-23-2021)

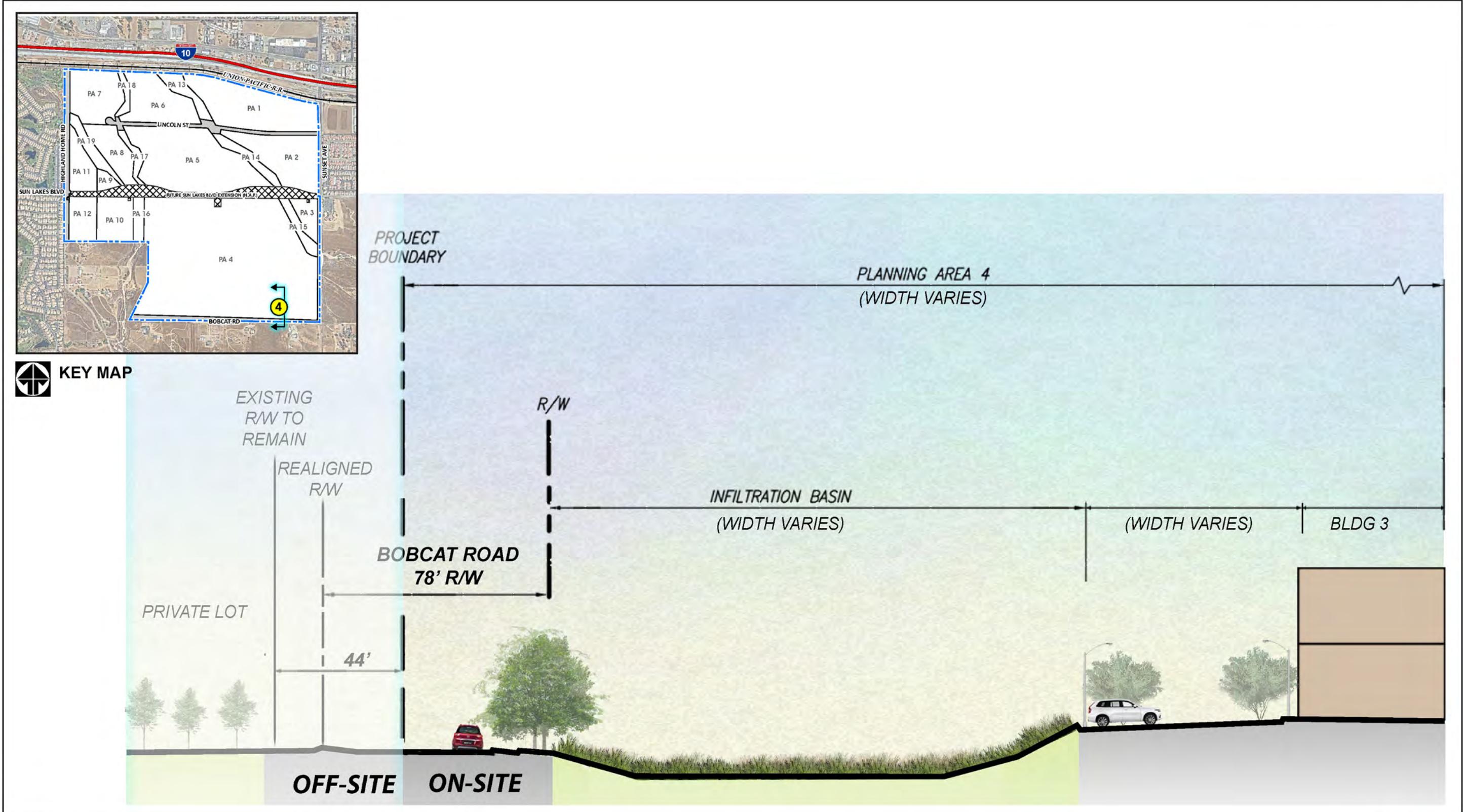
FIGURE 4-15



Source(s): Hunter Landscape (01-27-2023)

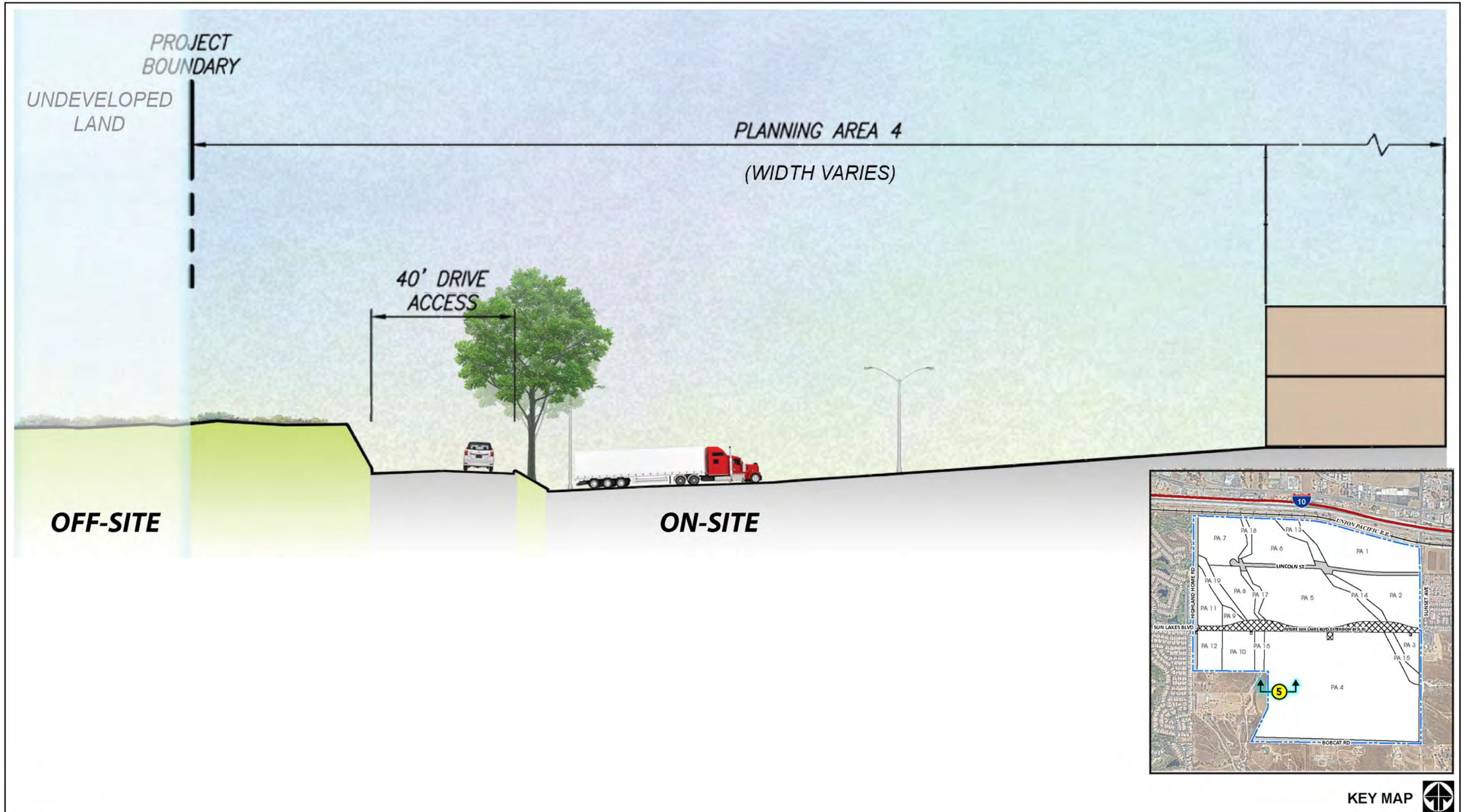
FIGURE 4-16





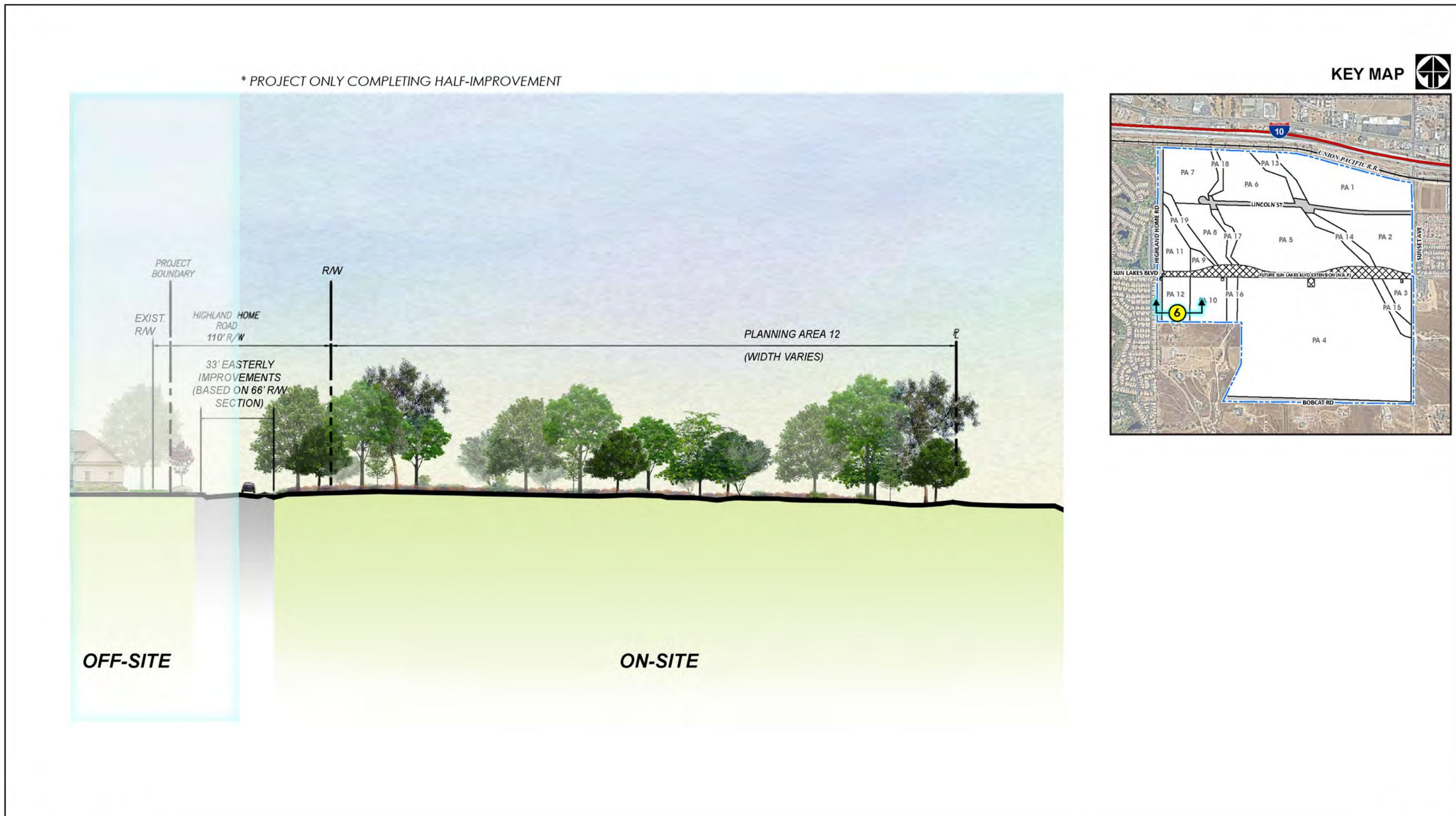
Source(s): Hunter Landscape (01-27-2023)

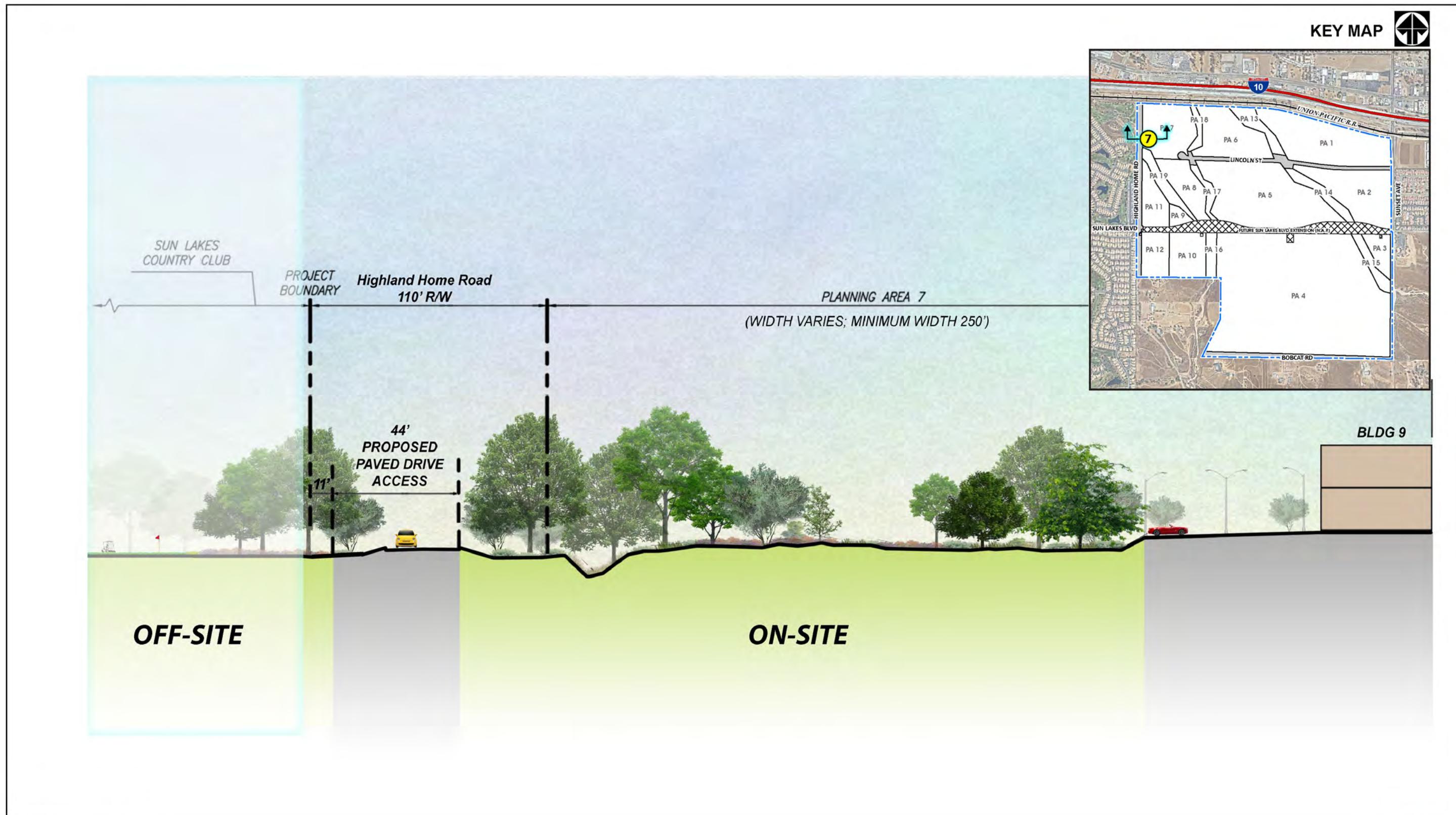
FIGURE 4-18



Source(s): Hunter Landscape (05-25-2021)

FIGURE 4-19





IMPLEMENTATION PLAN

5

CHAPTER FIVE presents the policies and procedures for the City of Banning's review and approval of implementing projects and describes the methods and procedures for interpreting and amending the Specific Plan as necessary.



- 5.1 IMPLEMENTATION OF SP 20-20000002
- 5.2 MODIFICATIONS TO THE SPECIFIC PLAN
- 5.3 MAINTENANCE PLAN
- 5.4 CONCEPTUAL PHASING PLAN

CHAPTER 5 | IMPLEMENTATION PLAN

5.1 IMPLEMENTATION OF SPECIFIC PLAN NO. 20-20000002

Approval of the SUNSET CROSSROADS Specific Plan (SP20-20000002) indicates acceptance by the City of Banning Planning Commission and City Council of the general framework of development for the 533.8-acre property described therein. Part of that framework establishes the Implementation Regulations which constitute the zoning regulations for the SUNSET CROSSROADS Specific Plan. The SUNSET CROSSROADS Specific Plan also includes Design Guidelines which establish the character of the project through its architecture and landscape design. Unlike the Implementation Regulations, the Design Guidelines are not compulsory, but are intended instead to provide clear direction while allowing flexibility during the design and review of implementing development projects. Further, it is anticipated that Specific Plan No. 20-20000002 will be implemented through a series of Design Reviews and Conditional Use Permits, which shall be reviewed and approved by the City of Banning. Approvals within the SUNSET CROSSROADS Specific Plan shall be subject to the review authority and review processes set forth in this Section.

5.1.1 APPLICABILITY

All development and proposed uses in the Specific Plan area shall comply with the Implementation Regulations set forth in this Specific Plan. Where conflicts exist between the Implementation Regulations contained in the SUNSET CROSSROADS Specific Plan and those found in the City's Zoning Ordinance, the standards in this Specific Plan shall take precedence. Implementation Regulations not addressed in this Specific Plan shall be subject to the applicable requirements of the City's Zoning Ordinance in effect at the time the submittal of an application for development within the Specific Plan or as otherwise provided in an approved and in-effect Development Agreement for the site. Unless otherwise provided for in a development agreement between the applicant and the City. In addition, procedures, notices, timelines, and appeals (including who is authorized to file appeals) not addressed in this Chapter 5, Implementation Plan shall be subject to applicable requirements of the Zoning Ordinance in effect at the time an application submitted within this Specific Plan is submitted. The Design Guidelines are not compulsory but are intended instead to provide clear direction while allowing flexibility during the design of implementing development projects. Consistency with the Design Guidelines of applications submitted within this Specific Plan shall be determined by the Community Development Director pursuant to Section 5.1.3.

5.1.2 SEVERABILITY

If any section, subsection, sentence, clause, or phrase of this Specific Plan, or future amendments or additions hereto, is for any reason held to be invalid or unconstitutional by the decision of any court of competent jurisdiction, such decision shall not affect the validity of the remaining portions of this Specific Plan.

5.1.3 INTERPRETATIONS

The Community Development Director may make interpretations related to the SUNSET CROSSROADS Specific Plan and/or the applicability of Zoning Ordinance requirements or, at his/her discretion, refer interpretations to the Planning Commission for its consideration and action. Interpretations made by the Community Development Director and/or decisions made by the Planning Commission may be appealed by the applicant to the Planning Commission, if applicable, and to the City Council per the procedures contained in the Zoning Ordinance in effect at the time an application for development within the Specific Plan is submitted, unless otherwise set forth in this Chapter 5 or any then in effect Development Agreement. The Community Development Director is authorized to make a determination

that a use not listed in Table 3-1 (*Permitted Uses*) shall be allowed by the Specific Plan because it is found to be substantially similar to, or no more objectionable than, an allowed use. Such a use shall be subject to the same review requirements of the "similar" allowed use. "Substantially similar" means that the use would be consistent with the purpose and intent of the Planning Area's Land Use Designation. Appeals to interpretations made by the Community Development Director or Planning Commission may be made by any applicant pursuant to Section 5.2.7. Implementation Regulations not addressed in this Specific Plan shall be subject to the applicable requirements of the City's Zoning Ordinance in effect at the time an application for development within the Specific Plan is submitted or as otherwise provided in an approved and effective Development Agreement for the site.

5.1.4 APPROVAL AUTHORITY

1. The Community Development Director shall be responsible for administering, interpreting, and enforcing all Implementation Regulations and Design Guidelines of the Specific Plan, including the acceptance and processing of all land use permit applications.

2. The Community Development Director is also the approval authority for the following applications subject to appeal to the Planning Commission per the procedures contained in the Zoning Ordinance in effect at the time the application is submitted, unless otherwise set forth in this Chapter 5 or any then effective Development Agreement:

- a. Review and approval of the Specific Plan Sign Program and Signage Design Guidelines;
- b. Modification to the Specific Plan Sign Program and Signage Design Guidelines;
- c. Occupancy Permits;
- d. Tenant Improvement Permits (except for Building Permits which remain governed by Ordinance);
- e. Lot Line Adjustments;
- f. Minor Modifications to the Specific Plan;
- g. Design Review - Administrative Review.

3. The Planning Commission is the approval authority for the following applications, subject to appeal to the City Council per the procedures contained in the Zoning Ordinance in effect at the time a project is submitted, unless otherwise set forth in this Chapter 5 or any then effective Development Agreement:

- a. Variances;
- b. Design Review - Planning Commission Public Hearing Required;
- c. Conditional Use Permits (subject to the procedures of the City's Zoning Ordinance);
- d. Minor Modifications to the Specific Plan with concurrent application as described in Section 5.2.3; and
- e. Any timely appeal by authorized parties of the matters described in Section 5.1.4.B.

The Planning Commission is the recommending body and the City Council is the final approval authority for General Plan Amendments, Specific Plan Amendments, Zone Changes, Zoning Ordinance Amendments, Development Agreements, and Subdivision including tentative and final maps, subject to the procedures set forth in the City's Zoning Ordinance.

5.2 REVIEW AND APPROVAL OF IMPLEMENTING DEVELOPMENT PROPOSALS

Implementation of development proposals shall be subject to review and approval by the City of the land use applications identified in Section 5.2.1 through 5.2.4 (as applicable) and the City's determination in accordance with Section 5.2.6 that its actions comply with the requirements of the California Environmental Quality Act (CEQA).

5.2.1 DESIGN REVIEW

Design Review, which is initiated by an application filed by an applicant, is one of the entitlement mechanisms for implementing development within this Specific Plan. A Design Review implements uses "permitted by right" including any ancillary uses pursuant to Table 3-1, *Permitted, Conditionally Permitted, Ancillary, and Prohibited Uses*, of this Specific Plan or other uses authorized by interpretation of the Specific Plan as described in Section 5.1.3, (Interpretations). Three (3) different Design Review processes may be used for implementing development within this Specific Plan, distinguished by the level of conformity with the Specific Plan and CEQA review requirements:

1. Design Review - Administrative Review:

A Design Review application which is found by the Community Development Director to be a "Permitted Use" or an "Ancillary Use" in compliance with the List of Permitted, Conditionally Permitted and Ancillary Uses in Table 3-1 or a use permitted without a Conditional Use Permit by interpretation as described in Section 5.1.3, (Interpretations) , shall be approved administratively by the Community Development Director, subject to Section 5.2.6 (Environmental Review) if the Community Development Director additionally finds that either:

- a. The project described in the application is consistent with the overall intent and/or character of the Specific Plan and the Design Guidelines; or
- b. The project described in the application is not in full compliance with other Implementation Regulations but is consistent with the overall intent and/or character of the Specific Plan and the Design Guidelines, and the Community Development Director additionally finds that: 1) that the proposed modifications are minor; and, 2) implementation of the proposed modifications would be generally consistent with the remaining Implementation Regulations, and with the overall intent and/or character of the Specific Plan and the Design Guidelines.

2. Design Review – Planning Commission Public Hearing:

A Design Review Application which is found by the Community Development Director to be a "Permitted Use" or an "Ancillary Use" in compliance with the List of Permitted, Conditionally Permitted and Ancillary Uses in Table 3-1 or a use permitted without a Conditional Use Permit by interpretation as described in Section 5.1.3, (Interpretations) but for which the Community Development Director cannot make the findings required by either Section 5.2.1.1.a. or Section 5.2.1.1 b., shall be referred by the Community Development Director to the Planning Commission for review and a determination at a public hearing. In addition, an application for Design Review shall require Planning Commission review and approval if the application includes a concurrent request for a Specific Plan Amendment or for a Minor Modification that is reviewable by the Planning Commission pursuant to Section 5.2.3. The Planning Commission may approve, condition or deny the Design Review application, or may determine that a Conditional Use Permit is required, or may determine that a Minor Modification or Specific Plan Amendment is additionally required. A Specific Plan Amendment would also require approval by City Council.

5.2.2 CONDITIONAL USE PERMITS

Uses subject to the approval of a Conditional Use Permit are identified in Table 3-1 (*Permitted, Conditionally Permitted, Ancillary, and Prohibited Uses*) of this Specific Plan with a "C" symbol. A Conditional Use Permit application is an intensive consideration of an application submitted within this Specific Plan; no Design Review application is required. Unless otherwise set forth in this Chapter 5, uses requiring the approval of a Conditional Use Permit shall be subject to the filing, required findings, notification, hearing, and appeal procedures identified in the Zoning Ordinance applicable

to Conditional Use Permits effective at the time of development permit application(s). Applications for Conditional Use Permits shall be subject to Section 5.2.6 (Environmental Review).

5.2.3 MINOR MODIFICATIONS

A Minor Modification may be used to propose minor changes to the Specific Plan text and graphics. Minor Modifications shall be reviewed and may be approved by the Community Development Director without a public hearing and shall be subject to Section 5.2.6 (Environmental Review). Any such approval shall be based on a finding made by the Community Development Director that the proposed changes to the Specific Plan text, graphics, and/or project design are minor deviations, adjustments, alterations, variations or changes which do not change the character or intent of the Specific Plan, and therefore do not require a Specific Plan Amendment.

However, a request for a Minor Modification accompanied by a concurrent application which requires Planning Commission and/or City Council review and approval, shall be reviewed and approved by the required hearing body for the concurrent application(s).

The following particular minor deviations, modifications or changes to the Specific Plan, along with other minor modifications that the Community Development Director may determine do not change the character or intent of the Specific Plan, shall be subject to review and administrative approval by the Community Development Director as a Minor Modification:

1. Modifications to the Specific Plan text and graphics which do not substantially change the character or intent of SUNSET CROSSROADS Specific Plan.
2. Modifications to the Specific Plan text or graphics resulting from any of the following:
 - a. Modifications to the right of way width, improvements and/or alignment of any roadway within the Specific Plan or bordering the Specific Plan that is requested or directed by any department of the City of Banning, or any other governmental or quasi-governmental agency.
 - b. Modifications to the location, size and/or timing of the installation of any sanitary sewer, storm drain, potable water or recycled water facility that is requested or directed by any department of the City of Banning, or any other governmental or quasi-governmental agency.
 - c. Addition, deletion or relocation of any public facility, requested or directed by any department of the City of Banning, including but not limited to fire station, electrical substation, water reservoir, reverse osmosis facility or any other type of facility intended to be owned or operated by the City of Banning or any other governmental or quasi-governmental agency.
3. Expansion or reduction of the net acreage of any Planning Area (and accompanying expansion or reduction of the net acreage covered by a given land use designation) provided that such change does not increase the size of any Planning Area by more than 20%.
4. Increases or decreases of no more than 20% to development intensity/density or building square footage within any or all of the Planning Areas, provided that the overall Maximum Building Square Footage for Industrial uses and for General Commercial uses within this Specific Plan is not exceeded. Maximum Building Square Footage is defined as the total square footage of all floors in structure as measured from either the interior surface of each exterior wall of the structure or, if the structure does not have walls, from each outer edge of the roof.
5. Construction of buildings across Planning Area boundaries with the same Land Use Designation, which cross over into abutting Planning Areas, subject to all of the applicable Implementation Regulations and Design

Guidelines of this Specific Plan. Buildings constructed across Planning Area boundaries or lot lines shall not, by themselves, trigger the requirement for a Specific Plan Amendment.

6. Modifications to landscape coverage that result in reductions in coverage of less than 25% of that stated within the Specific Plan.
7. Modifications to the SUNSET CROSSROADS signage requirements contained in this Specific Plan or to any subsequently approved Specific Plan Sign Program and Signage Design Guidelines.
8. Modifications to the heights of walls and fences by no more than 1 foot.
9. Decrease in parking requirements of not more than 10 spaces with a parking study reviewed and approved by the Community Development Director. Applications submitted within this Specific Plan shall be consistent with parking requirements set forth in the Zoning Ordinance Section 17.28.
10. Other minor changes, deviations, adjustments, alterations, variations or modifications of a similar nature to those listed above or which are deemed minor by the Community Development Director, including (i) minor changes, adjustments, alterations, variations, deviations or modifications to landscape materials, wall materials, wall alignment, entry design, streetscape design, and the lengths and/or location of streets not identified in the General Plan Circulation Element and or pursuant to modifications on the final map and (ii) minor modifications to the Implementation Regulations (except for the Permitted, Conditionally Permitted, and Ancillary Uses) and Design Guidelines set forth in this Specific Plan, provided such changes, adjustments, alterations, variations, deviations or modifications are consistent with the intent of the Implementation Regulations and Design Guidelines and with the character and intent of the Specific Plan, and in substantial conformance with the City of Banning General Plan.

5.2.4 SPECIFIC PLAN AMENDMENTS

All Specific Plan modifications which do not meet the criteria of a Minor Modification as defined in Section 5.2.3 shall be deemed to require a Specific Plan Amendment. When public hearings are authorized or required to adopt Amendments to the Specific Plan, notices of public hearings shall be provided, and hearings held in accordance with the relevant procedural provisions of the Zoning Ordinance then in effect. Specific Plan Amendments shall be processed in accordance with the applicable provisions of State law provided in California Government Code section 65450 et. seq. and shall be subject to the review and approval of the City Council. The Planning Commission shall first hear and consider applications for Specific Plan Amendments and provide a recommendation to the City Council.

Any Specific Plan Amendment initiated by an applicant requires the filing of a City application and required materials supporting the amendment, submittal of a fee deposit, Planning Commission review and recommendations, and City Council review and final decision.

5.2.5 SPECIFIC PLAN SIGN PROGRAM

Prior to the approval of the first application submitted within this Specific Plan, a comprehensive Specific Plan Sign Program and Signage Design Guidelines for the SUNSET CROSSROADS Specific Plan consistent with the Signage Design Guidelines (Section 4.5) shall be prepared and reviewed pursuant to Section 5.1.4 (*Approval Authority*). The Specific Plan Sign Program and Signage Design Guidelines shall establish provisions for the development and use of signs within the Specific Plan. The Specific Plan Sign Program and Signage Design Guidelines shall exclusively regulate the development and use of all signs within the Specific Plan and shall take precedence over the provisions of the Zoning Ordinance related to signs (Section 17.36.110 - *Sign regulations*).

5.2.6 ENVIRONMENTAL REVIEW

The SUNSET CROSSING Specific Plan and all subsequent entitlements for the Project shall be subject to CEQA. For purposes of CEQA, the SUNSET CROSSROADS Specific Plan is considered a "project." The evaluation of environmental impacts under CEQA for the SUNSET CROSSROADS Specific Plan is contained in the project's Environmental Impact Report, a project level environmental impact report certified by the City of Banning concurrently with the approval of the SUNSET CROSSROADS Specific Plan (EIR). Adoption of the SUNSET CROSSROADS Specific Plan, for which an environmental determination shall be made, and mitigation measures adopted, shall be deemed to have satisfied the environmental review requirements for build out of the Specific Plan area. Because the Project has been extensively reviewed in the EIR, it is the intent of the City in adopting this Specific Plan that applications for implementing entitlements for the Project, including with respect to grading, improvements, individual maps, buildings or phases, consistent with this Specific Plan shall not require additional review under CEQA, provided that none of the conditions are present which require further environmental review under CEQA, in particular, Public Resources Code Section 21166.

Upon filing of any application by applicant, a review in compliance with CEQA shall be conducted by the City to determine potential environmental impacts resulting from each land use application to implement the Specific Plan, if any, and if said proposal is consistent with the EIR, does not require subsequent environmental documentation, or is exempt from the provisions of CEQA. The CEQA review shall be undertaken as part of the review process for applications submitted within this Specific Plan. The Community Development Director shall make the determination as to the level of CEQA documentation (exemption, addendum, supplemental EIR, supplemental MND, or subsequent EIR or no further review required), that is appropriate and may request and obtain technical information as necessary to make this determination.

Although the EIR analyzes a range of uses as part of a conceptual development plan, it is recognized that the intent of the Specific Plan is not to identify a particular mixture or intensity of land uses, but rather to designate areas where a mixture of commercial or industrial uses may be developed. To permit development of the full range of uses described by the Specific Plan, any use shall be considered consistent with the intent of the Specific Plan if the City, in considering an application for a specific permitted use, conditionally permitted use or ancillary use, determines that no significant environmental impacts not addressed in the EIR or subsequent environmental documentation result from such proposed use.

5.2.7 APPEALS

Notwithstanding any other provision of this Specific Plan or the Zoning Ordinance, appeals from any determination of the Community Development Director and/or Planning Commission with respect to all matters may be made on forms provided by the City of Banning, and accompanied by the appropriate filing fee, where applicable, within fifteen (15) days following the final date of action for which an appeal is made. Except as specifically set forth in this Chapter 5, appeals may be brought and shall be processed consistent with the Zoning Ordinance.

5.3 MAINTENANCE PLAN

Successful operation of maintenance entities and maintenance associations are important in maintaining the quality of a development. The public and private improvements constructed within SUNSET CROSSROADS shall be maintained through a combination of public and private entities as described in Table 5-1, *Maintenance Responsibilities*. A Master Property Owners' Association (MPOA) and/or Property Owner's Association (POA) shall be established for the maintenance of privately-owned common area landscape improvements, storm water and water quality management facilities, and private driveways within areas of SUNSET CROSSROADS. Indication of more than one party responsible for maintenance of any facility on Table 5-1, infers that maintenance may be undertaken by one party or any

combination of the listed parties, subject to a formal agreement. For areas in public ownership (such as public roadway ROWs), maintenance districts may fund the maintenance of these areas.

5.4 CONCEPTUAL PHASING PLAN

The SUNSET CROSSROADS Specific Plan is designed for development in response to market demands and according to the logical and orderly extension of roadways, public utilities, and infrastructure. The exact timing of implementation for any Planning Area may vary based on several factors, including market and economic demands, as well as physical constraints or timing of infrastructure improvements. It is anticipated three different types of phasing would occur to implement the Specific Plan: 1) Grading, 2) Mapping, and 3) Infrastructure Construction and Development. Development of the Planning Areas may take place in any order.

GRADING

Phasing of the grading may occur in two (2) or more phases and in any order: 1) north of the SLB Extension, and 2) south of the SLB Extension, and/or may occur in smaller increments, subject to approved Grading Plans and Permits.

MAPPING

Phasing of the mapping may occur with multiple Tentative Parcel Maps anticipated to be processed for the Specific Plan.

INFRASTRUCTURE CONSTRUCTION AND DEVELOPMENT

Planning Areas may be developed in any sequence, or increment, provided that the infrastructure improvements required to serve the implementing development are available at the time of development or constructed concurrently with the development. Planning Areas which are dependent on adjacent Planning Areas for access shall demonstrate the ability to provide the necessary infrastructure and access, including the construction of interim infrastructure, prior to issuance of building permits. Applications submitted within this Specific Plan s may be approved by the City of Banning Planning Department, provided vehicular access, public facilities and infrastructure is constructed to adequately service the development or as needed for public health and safety in each stage of development.

An agreement with the City of Banning or other capable service provider shall be made in writing which states that the provision of services to any application submitted within this Specific Plan shall be available prior to the recordation of any subdivision maps.

TABLE 5-1 MAINTENANCE RESPONSIBILITIES

| FACILITY | MASTER PROPERTY OWNERS' ASSOCIATION | PROPERTY OWNER OR OCCUPANT | CITY OF BANNING | OTHER MAINTENANCE ENTITY ¹ |
|--|-------------------------------------|----------------------------|-----------------|---------------------------------------|
| CIRCULATION & RELATED FACILITIES | | | | |
| Public Roadways | | | X | |
| Pavement & Curbs | | | | |
| Landscaping within public right-of-way, including medians and parkways | | | X | |
| Sidewalks | | | X | |
| Private Driveways and Drive Aisles | X | X | | |
| Parking Lots, including landscaping | X | X | | |

| FACILITY | MASTER PROPERTY OWNERS' ASSOCIATION | PROPERTY OWNER OR OCCUPANT | CITY OF BANNING | OTHER MAINTENANCE ENTITY ¹ |
|---|-------------------------------------|----------------------------|-----------------|---------------------------------------|
| Traffic Signals ² | | | X | |
| Traffic Signs | | | | |
| Within public right-of-way | | | X | |
| Outside public right-of-way | X | | | |
| Street Lights | | | | |
| Within public right-of-way | | | X | |
| Outside public right-of-way | X | | | |
| UTILITY INFRASTRUCTURE | | | | |
| Potable Water Facilities/Infrastructure | X | | X | |
| Reclaimed Water Facilities/ Infrastructure | X | | X | |
| Sanitary Sewer Facilities/Infrastructure | X | | X | |
| Storm Water Drainage Facilities/ Infrastructure | X | | | X |
| Dry Utilities (Natural Gas, Communications Systems) | | | | X |
| Dry Utilities (Electricity) | | | X | |
| LANDSCAPING, OPEN SPACE, & RELATED FACILITIES | | | | |
| Open Space-Resource Planning Areas 13 through 19 (Existing Natural Drainage Courses) | X | | | X |
| Planning Area 11 (Park Site) | | | X | |
| Planning Area 11 (Open Space/Trails) | X | | | |
| Planning Area 12 (Open Space-Resource/Trails/ Open Space) | X | | | |
| Common area landscaping, including entry treatments | X | | | X |
| Entry Monumentation, Freeway Oriented Freestanding Signs, and Other Signage | X | X | | |
| Walls and Fences | X | X | | |
| Outdoor Employee Patio Areas | X | X | | |
| Outdoor Plazas/Gathering Areas | X | X | | |
| Notes: | | | | |
| 1. Other Maintenance Entities may include utility providers, resource agencies, City Departments, park districts, flood control district, Landscape, Lighting and Maintenance District (LLMD), and other public/private entities. 2. Traffic signals and utility facilities/infrastructure may require maintenance easements. | | | | |
| 3. The existing steel pole electrical power line shall be protected in place at the northern boundary via a 20' easement. | | | | |

END OF DOCUMENT